

# Agenda

## Planning Committee

Wednesday, 24 November 2021 at 7.30 pm

New Council Chamber, Town Hall, Reigate



This meeting will take place in accordance with Government guidance. The Committee will assemble at the Town Hall, Reigate. Members of the public, Officers and Visiting Members should attend remotely.

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### Members:

#### S. Parnall (Chairman)

M. S. Blacker  
G. Adamson  
J. Baker  
Z. Cooper  
R. Harper  
A. King  
F. Kelly

J. P. King  
S. A. Kulka  
S. McKenna  
R. Michalowski  
R. Ritter  
C. Stevens  
S. T. Walsh

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Published 16 November 2021

## Substitutes:

<b>Conservatives:</b>	R. Absalom, A. C. J. Horwood, J. Hudson, M. Tary and R. S. Turner
<b>Residents Group:</b>	R. J. Feeney, P. Harp, N. D. Harrison and C. T. H. Whinney
<b>Green Party:</b>	J. Booton, P. Chandler, J. C. S. Essex, S. Sinden and D. Torra
<b>Liberal Democrats</b>	M. Elbourne

**Mari Roberts-Wood**  
Interim Head of Paid Service

**1. Minutes** (Pages 5 - 10)

To confirm as a correct record the Minutes of the previous meeting.

**2. Apologies for absence**

To receive any apologies for absence.

**3. Declarations of interest**

To receive any declarations of interest.

**4. Addendum to the agenda** (To Be Tabled)

To note the addendum tabled at the meeting which provides an update on the agenda of planning applications before the Committee.

**PLANNING APPLICATIONS:**

**NOTES:**

1. The order in which the applications will be considered at the meeting may be subject to change.
2. Plans are reproduced in the agenda for reference purposes only and are not reproduced to scale. Accordingly dimensions should not be taken from these plans and the originals should be viewed for detailed information. Most drawings in the agenda have been scanned, and reproduced smaller than the original, thus affecting image quality.

To consider the following applications :

**5. 21/01458/F - Hockley Industrial Centre, Hooley Lane, Redhill** (Pages 11 - 66)

Partial demolition of existing buildings and erection of four apartment blocks comprising 68 dwellings with associated parking and landscaping. As amended on 18/10/2021.

**6. 21/01367/F - 136-140 Brighton Road, Burgh Heath** (Pages 67 - 94)

Demolition of 138 and erection of 1 bungalow, 1 semi-detached house and 8 maisonettes with associated access, parking and amenity space.

- 7. a) 20/02233/F and b) 20/01217/LBC - The Bulls Head Public House, 55 High Street, Reigate** (Pages 95 - 112)

External works to rear garden including the erection of an open sided timber pergola with felt roof finish, formation of a new decked area, installation of timber posts supporting stretched canvas sail to new decked area, construction of new external bottle bar timber shed.

- 8. 18/02159/F - Land Parcel at Gower Road, Horley** (Pages 113 - 138)

Erection of 2 semi-detached houses. As amended on 27/02/2019 15/04/2019, 01/08/2019 and 13/07/2021.

- 9. 21/02537/ADV - Harlequin Theatre, 16 London Road, Redhill** (Pages 139 - 148)

Vinyl wrapping of a staircase, glass and on windows at the atrium of the Harlequin Theatre.

- 10. Any other urgent business**

To consider any item(s) which, in the opinion of the Chairman, should be considered as a matter of urgency.



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As we would all appreciate, our meetings will be conducted in a spirit of mutual respect and trust, working together for the benefit of our Community and the Council, and in accordance with our Member Code of Conduct. Courtesy will be shown to all those taking part.



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**Notice is given** of the intention to hold any part of this meeting in private for consideration of any reports containing "exempt" information, which will be marked accordingly.

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## **BOROUGH OF REIGATE AND BANSTEAD**

### **PLANNING COMMITTEE**

Minutes of a meeting of the Planning Committee held at the New Council Chamber - Town Hall, Reigate on 3 November 2021 at 7.30 pm.

Present: Councillors S. Parnall (Chairman), M. S. Blacker (Vice-Chair), G. Adamson, J. Baker, Z. Cooper, R. Harper, A. King, F. Kelly, J. P. King, S. A. Kulka, S. McKenna, R. Ritter, C. Stevens, R. Absalom (Substitute) and J. Hudson (Substitute).

Also present: Councillor Sachdeva .

#### **40. MINUTES**

It was **RESOLVED** that the minutes of the meeting held on 29 September 2021 be approved as a correct record.

#### **41. APOLOGIES FOR ABSENCE**

Apologies for absence had been received from Councillors Michalowski and Walsh, Councillors Hudson and Absalom attended at their respective substitutes.

#### **42. DECLARATIONS OF INTEREST**

There were none.

#### **43. ADDENDUM TO THE AGENDA**

**RESOLVED** that the addendum be noted.

#### **44. 21/00823/F - 85, 87 AND 89, BRIGHTON ROAD, REDHILL**

The Committee considered an application at 85, 87 and 89, Brighton Road, Redhill for the demolition of existing buildings and erection of an apartment building comprising 19 flats with commercial use at ground floor plus associated car parking.

Jeremy Woolley, a member of the public, spoke in objection to the application stating he lived in a property to the south of the site. The report stated that the element facing the cottages was only 2 storeys tall and thus there would be no adverse impact. However, the main view from the living spaces was of the 4-storey part of the plans, resulting in a significant loss of outlook and daylight compared to the existing building. There would be loss of privacy as the main living spaces would be overlooked by 8 balconies and 6 windows across 8 different flats. It was felt that there had been a lack of consultation and public notice of the proposals. The site should be improved however the Committee was urged to consider whether this development was appropriate.

Phillip Russell, the agent, spoke in support of the application, explaining that work had been carried out with officers on the scheme for 3 years, having listened to objections. Much had been done to address the issues of overlooking and the development was in accordance with Planning Policy. There was a duty to maximise potential of all sites, particularly brownfield town centre sites, to protect the greenfield and greenbelt sites and to meet the demand for housing. The

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proposal struck a good balance of maximising the site which would give a mix of high quality private and affordable homes and contributing towards the rejuvenation of the local area. Particular aspects of this scheme were that it was compliant with the policy requirements for Affordable Housing and residential parking spaces; the dwellings per hectare were greater than two recently adjacent approved flatted schemes; and the existing premises were in a dilapidated state.

A reason for refusal was proposed by Councillor Ritter and seconded by Councillor McKenna, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

1. The proposal, by reason of the significant height, width, depth of the proposed building, which extends across the entire width and depth of the site and sits above the neighbouring buildings, the bulky roof design, and the unsympathetic design which fails to reflect the form and character of the neighbouring buildings, would be incongruous and cramped overdevelopment of the site, out of keeping with and harmful to the character and appearance of the locality contrary to policies DES1 of the Reigate and Banstead Development Management Plan 2019 and chapter 12 of the NPPF.
2. The proposal, by reason of its height, depth, bulk, massing and proximity to the dwellings located to the west (Cleveland Cottage and 99A Garlands Road) and north (1-3 Stile Cottages) would appear as a dominant and overbearing structure when viewed from these neighbouring properties as well as result in overlooking and loss of privacy to these dwellings. The proposal would therefore adversely impact the residential amenities of their occupants contrary to policy DES1 of the Reigate and Banstead Development Management Plan 2019.

## 45. 20/01791/F - 40 HOLMETHORPE AVENUE, REDHILL

The Committee considered an application at 40 Holmethorpe Avenue, Redhill, for the demolition of existing office, new build of 3-storey office building with car parking at ground level with external landscaping. As amended on 12/02/2021 and on 10/08/2021.

**RESOLVED** that planning permission be **GRANTED** subject to conditions.

## 46. 21/00679/OUT - SUBUD HALL, ALLINGHAM ROAD, REIGATE

The Committee considered an outline application at Subud Hall, Allingham Road, Reigate, for the use of the site for two residential dwellings.

A reason for refusal was proposed by Councillor J King and seconded by Councillor Baker, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

1. The proposed loss of the existing community facility would result in a shortfall of such facilities in the local area which, without sufficient marketing evidence to demonstrate that its continued community use is unviable, would be contrary to Policy CS12 of the Reigate and Banstead Borough Council Core

Strategy 2014 and Policy INF2 of the Reigate and Banstead Development Management Plan 2019.

**47. 21/ 00525/RET - BATTLEBRIDGE SPORTS GROUND, BATTLEBRIDGE LANE, MERSTHAM**

The Committee considered an application at Battlebridge Sports Ground, Battlebridge Lane, Merstham for retrospective consent for the installation of a container for storage on land associated to Battlebridge Athletics Track, Battlebridge Lane. Container previously granted temporary consent in 2015.

**RESOLVED** that retrospective planning permission be **GRANTED** subject to conditions and as per addendum changes.

**48. 21/02358/HHOLD - 31 ASHDOWN ROAD, REIGATE**

The Committee considered an application at 31 Ashdown Road, Reigate for a two storey and single storey extensions.

**RESOLVED** that planning permission be **GRANTED** subject to conditions.

**49. DEVELOPMENT MANAGEMENT QUARTER 2 - 2021-22 PERFORMANCE**

The Head of Planning explained that the number of applications received and determined had been the focus of the previous quarterly report as these had been at record levels. There were concerns regarding sustainability, however it was stated that the number of applications had reduced to a more manageable level and if levels remained the same as in quarter 2 concerns over sustainability within the Planning Team could be allayed.

There had been 344 house holder applications in quarter 2, this did not include lawful development applications. A large number of applications had been determined and currently there was no backlog. Overall, performance within the department had held up well in quarter 2 and this was explained. It was noted that it was currently taking 3.3 days to register an application and the Committee concurred this was exceptional.

One major appeal had been dismissed in quarter 2 and all major appeals had been dismissed in the last 4 quarters. For non-major appeals the department was on target for this year.

In terms of enforcement, there had been a large number of potential breaches reported. The Enforcement Team had been temporarily reduced from 4 members of staff to 3, however once the Team was back to full capacity this would ease the pressure.

The Planning Team was thanked for their continuing work.

**50. ANY OTHER URGENT BUSINESS**

There was none.

**51. EXEMPT BUSINESS**

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**RESOLVED** that members of the Press and public be excluded from the meeting for part of agenda item 13 (Confidential address in Redhill) under Section 100A(4) of the Local Government Act 1972 on the grounds that:

- i. it involves the likely disclosure of exempt information as defined in paragraphs 1 and 6a) and 6b) of Part 1 of Schedule 12A of the Act;
- ii. 1. Information relating to any individual.
- iii. 6. Information which reveals that the authority proposes –
  - (a) to give under any enactment a notice under or by virtue of which requirements are imposed on a person: or
  - (b) to make an order or direction under any enactment.

## 52. **CONFIDENTIAL ADDRESS IN REDHILL**

**RESOLVED** that the Committee considered the information in the exempt report.

The Meeting closed at 10.10 pm

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 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	24 <sup>th</sup> November 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Michael Parker
	<b>TELEPHONE:</b>	01737 276339
	<b>EMAIL:</b>	Michael.parker@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	5	<b>WARD:</b> Earlswood And Whitebushes

<b>APPLICATION NUMBER:</b>	21/01458/F	<b>VALID:</b>	28/06/2021
<b>APPLICANT:</b>	Nordhus Properties	<b>AGENT:</b>	Clemebil Ltd
<b>LOCATION:</b>	<b>HOCKLEY INDUSTRIAL CENTRE HOOLEY LANE REDHILL SURREY RH1 6ET</b>		
<b>DESCRIPTION:</b>	<b>Partial demolition of existing buildings and erection of four apartment blocks comprising 68 dwellings with associated parking and landscaping. As amended on 18/10/2021.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

The site is a wedge shaped piece of land, located between Hooley Lane, Woodlands Avenue and the railway line to the south of Redhill. The site is now vacant but was previously used for a variety of general industrial uses including motor vehicle repairs and storage and is occupied by four principal buildings. Hooley Lane rises in level quite sharply along the frontage of the site from a low point under the railway bridge. Three of the buildings upon the site have some historic merit, with the main and larger building fronting onto Hooley Lane not considered of merit. The site has a previous extant permission for residential redevelopment.

Adjacent to the site is a terrace of Victorian properties – Brighton Terrace - located just off Woodlands Avenue with pedestrian access onto Hooley Lane. Woodlands Avenue itself is a more eclectic mix of predominantly Victorian era detached and semi-detached properties. There is some variety surrounding the site ranging from recent new build flatted development, the Marquis of Granby pub and the more open, green areas of Redhill Common to the west of the application site.

This is a full application for the Partial demolition of existing buildings and erection of four apartment blocks comprising 68 dwellings with associated parking and landscaping. The proposed mix would be 26x1bed flats and 38x 2 bed market units and 4x1bed affordable units.

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This revised application follows the earlier approval on the site (18/00967/OUT and subsequent section 73 application) for redevelopment to provide 60 residential units. The proposals would be broadly similar in nature, access, layout, scale and design of buildings to the previously approved scheme (18/00967/OUT) with four blocks proposed. Block 4 (6 units) and Block 1 (the largest at 34 units) would be located at the northern end of the site and front on to Holley Lane. Block 2 (19 units) and Block 3 (9 units) would be located to the southern end of the site adjacent to the railway line to the east. To the west of the site, and block 2 and 3 are 1-15 Brighton Terrace and residential properties which front on to Woodlands Avenue.

The proposed mix and level of affordable housing is considered acceptable. In the previous application 3 affordable units were negotiated with the open book viability appraisal demonstrating that no more could be achieved. An additional unit has been negotiated with this revised application which equates to a provision of a higher percentage than previously and is considered acceptable without the need for a revised open book viability appraisal, given that doing so would risk the possibility of lower provision. A claw back mechanism would be included within the S106 (legal agreement) to secure any unexpected uplift in value following completion.

In terms of the design and scale of the scheme whilst the proposal would include 8 extra units the layout, scale and massing would be very similar to the approved 2018 scheme and in fact results in a slight reduction in footprint. The amended design of the proposed blocks is considered to be an improvement on the extant scheme in both visual appearance and the improvement to the quality of the accommodation with the addition of balconies and larger windows to the main living rooms.

The proposal is considered to have an acceptable relationship to the surrounding residential properties with the impact being very similar to the extant scheme. Conditions are recommended in relation to windows for both blocks 3 and 4 and balconies for block 3 to prevent unacceptable loss of privacy.

Subject to conditions the scheme is considered acceptable with regard to quality of accommodation for future residents, contamination, drainage, ecology, trees, crime, and sustainable construction.

As with the previous, the scheme would not meet the Council's adopted minimum parking standards with only 53 parking spaces. However, the following factors need to be taken in to account:

- The scheme would provide the same ratio of parking spaces when compared to the 2018 scheme (0.78 spaces per unit);
- the site is considered sustainable with regard to access to local services and public transport;
- car ownership in flatted schemes within the surrounding area is shown to be lower than the required 1 space per unit (0.56 - 0.84 cars per unit);
- there is another recent permission 19/00210/OUT in Brook Road which had a ratio 0.65 spaces per unit; and
- no objection has been raised by Surrey County Council in relation to highway safety due to parking restrictions in the surrounding area.

Having regard to the above factors and the fallback position offered by the extant scheme, the non-compliance with parking standards is considered to not result in any additional unacceptable harm.

In conclusion the scheme is found to be compliant with the requirements of the development plan and would make efficient use of a brownfield urban site whilst adhering closely to the layout, scale, height and massing previously approved scheme. The current application would secure a viable and deliverable scheme which would enable the re-use of this brownfield site. The benefits of which would be a greater contribution to local housing need, and all of the associated social and economic benefits which flow from that and additional affordable housing compared to the consented proposals. The application would also secure the retention and restoration of the heritage assets on the site. These benefits must be weighed against any potential harm identified.

It is therefore the view of officers that there are no substantive grounds to refuse the application and as such it is recommended for approval.

## **RECOMMENDATION(S)**

Subject to the completion of all documentation required to create a planning obligation under Section 106 of the Town and Country Planning Act 1990 (as amended to secure:

- (i) 4 units of affordable housing as shared ownership tenure;
- (ii) a post-completion clawback mechanism whereby the Council would receive a percentage of any uplift in the Gross Development Value (GDV) as a further contribution towards affordable housing
- (iii) The Council's legal costs in preparing the agreement

Planning permission is **GRANTED** subject to conditions.

In the event that a satisfactorily completed obligation is not received by 24 February 2022 or such longer period as may be agreed, the Head of Places and Planning be authorised to refuse permission for the following reason

1. The proposal fails to provide an agreed contribution to fund affordable housing provision within the Borough of Reigate & Banstead, and is therefore contrary to policy DES6 of the Reigate and Banstead Development Management Plan 2019.

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## Consultations:

Highway Authority: The County Highway Authority has assessed the application on safety, capacity and policy grounds and has raised no objection subject to conditions.

Environmental Health (Contaminated Land): recommends contaminated land conditions

Network Rail: Welcomes that the revised planning application retains the existing Network Rail access to the railway line for maintenance and emergency purposes. It is critical to the operational railway that this access point remain unobstructed. Recommend that contact was made with the Asset Protection team.

Surrey County Council Lead Local Flood Authority: Satisfied that the proposed drainage scheme meets the national guidance and technical standards. Condition recommended to secure further finalised details of drainage strategy and implementation of drainage strategy.

Surrey Police Designing Out Crime Officer: recommends a Secure by Design condition.

## Representations:

Letters were sent to neighbouring properties on 2<sup>nd</sup> July 2021 and a site notice was posted 9<sup>th</sup> July 2021 and advertised in local press on 15 July 2021.

9 responses have been received raising the following issues:

<b>Issue</b>	<b>Response</b>
Noise & disturbance	See paragraphs 6.27 to 6.34
Overshadowing	See paragraphs 6.27 to 6.34
Overlooking and loss of privacy	See paragraphs 6.27 to 6.34
Overbearing relationship	See paragraphs 6.27 to 6.34
Out of character with surrounding area	See paragraphs 6.4 to 6.10
Overdevelopment	See paragraphs 6.4 to 6.10
Poor design	See paragraphs 6.4 to 6.10
Harm to listed building and loss of railway buildings	See paragraphs 6.4 to 6.10
Increase in traffic and congestion	See paragraphs 6.35 to 6.45
Concern regarding access to Brighton Terrace	See paragraphs 6.35 to 6.45
Inadequate parking	See paragraphs 6.35 to 6.45

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Drainage and sewage capacity	See paragraphs 6.50 to 6.51
Harm to wildlife habitat	See paragraphs 6.55 to 6.59
Crime fears	See paragraph 6.64 to 6.66
Impact on/lack of infrastructure and facilities/amenities in local area to support increased population	See paragraphs 6.67 to 6.68
Loss of/harm to trees	See paragraphs 6.60 to 6.63
Loss of private view	Not a material planning consideration
Health fears	See paragraphs 6.27 to 6.34 and 6.52 to 6.54
No need for the development	Each scheme must be assessed on its own planning merits
Alternative scheme preferred	Submitted scheme must be assessed on its own planning merits

## **1.0 Site and Character Appraisal**

- 1.1 The site is a wedge shaped piece of land, located between Hooley Lane, Woodlands Avenue and the railway in Redhill. The site is now vacant but was previously used for a variety of general industrial uses including motor vehicle repairs and storage and is occupied by four principal buildings.
- 1.2 Hooley Lane rises in level quite sharply along the frontage of the site from a low point under the railway bridge. As a result, part of the site is elevated quite significantly above the road level and there is a high part concrete, part brick retaining wall along the Hooley Lane frontage which dominates the street scene.
- 1.3 Three of the buildings upon the site have some historic merit, with the main and larger building fronting onto Hooley Lane not considered of merit. The small building at the front of the site and the building present employed for coach repairs have some historic merit by reason of their siting, brickwork, arches and detailing, whilst the building located at the rear is a locally listed building. Examining the historic maps, it appears that the buildings are not original station buildings, but some may date from 1840-1860's.
- 1.4 The rear most building is locally listed. Following a review during the previous planning applications, it was concluded the goods shed running down the side of the site is the most important historic building on the site, likely to have been built sometime between 1845 and 1860, after the first Redhill railway station of 1841 had closed on the site in 1844.
- 1.5 Adjacent to the site is a terrace of Victorian properties – Brighton Terrace - located just off Woodlands Avenue with pedestrian access onto Hooley Lane.

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Woodlands Avenue itself is a more eclectic mix of predominantly Victorian era detached and semi-detached properties. There is some variety surrounding the site ranging from recent new build flatted development, the Marquis of Granby pub and the more open, green areas of Redhill Common to the west of the application site.

1.6 As a whole, the application site extends to approximately 0.52ha.

## 2.0 Added Value

2.1 Improvements secured at the pre-application stage: The applicant did not approach the Council for pre-application advice in relation to this scheme but did with regard a proposal for an enlarged Block 1. This led to the advice given that any application would need to be comprehensive and ought to provide an appropriate level of car parking.

2.2 Improvements secured during the course of the application: Amended site plan provided to add 4 further parking spaces. Amended plans for Block 3 provided showing obscure glazing of first floor and above west facing windows.

2.3 Further improvements to be secured through planning conditions or legal agreement: Various conditions are recommended to control materials, details and landscaping to ensure a high quality development. A legal agreement will be required to secure the on-site affordable housing provision. A condition is also proposed to secure salvage of elements of the Goods Station eastern elevation. Condition to secure uncontrolled pedestrian crossing on Hooley Lane.

## 3.0 Relevant Planning and Enforcement History

3.1	20/00812/S73	Outline planning application for the partial demolition of existing buildings, erection of 4 apartment blocks comprising 23 x 1 bed flats and 37 x 2 bed flats (60 in total). Variation of condition 1 of 18/00967/OUT amendment to approved plans - minor adjustment to the site boundary (red line) along the eastern boundary, minor changes to building footprints, internal reconfiguration of apartment blocks and changes to elevational design and materiality to all buildings. As amended on 08/06/2020 and on 19/06/2020	Approved with conditions 22 July 2020
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- |     |              |  |  |
|-----|--------------|--|--|
| 3.2 | 18/00967/RM1 | Submission of Reserved Matters for landscaping of Outline planning application for the partial demolition of existing buildings, erection of 4 apartment blocks comprising 23 x 1 bed flats and 37 x 2 bed flats (60 in total) granted under 18/00967/OUT. | Approved with conditions<br>19 November 2019 |
| 3.3 | 18/00967/OUT | Outline planning application for the partial demolition of existing buildings, erection of 4 apartment blocks comprising 23 x 1 bed flats and 37 x 2 bed flats (60 in total) - Approved with conditions  | Approved with conditions<br>2 May 2018       |
| 3.4 | 15/01008/OUT | Partial demolition of existing buildings, erection of 4 apartment blocks comprising 33 x 2 bed and 16 x 1 bed apartments, retention of Locally Listed wall and conversion of existing building into 2 bed house.   | Refused<br>Appeal allowed<br>8 May 2017      |
- 3.5 In addition, there is history associated with the business/industrial use of the site; however, this is not felt to be pertinent to wholesale redevelopment of the site.

## 4.0 Proposal and Design Approach

- 4.1 This is a full application for the Partial demolition of existing buildings and erection of four apartment blocks comprising 68 dwellings with associated parking and landscaping. The proposed mix would be 26x1bed flats and 38x 1 bed market units and 4x1bed affordable units.
- 4.2 This revised application follows the earlier approval on the site (18/00967/OUT and subsequent section 73 application) for redevelopment to provide 60 residential units. The proposals would be broadly similar in nature, access, layout, scale and design of buildings to the previously approved scheme (18/00967/OUT) with four blocks proposed. Block 4 (6 units) and Block 1 (the largest at 34 units) would be located at the northern end of the site and front on to Holley Lane. Block 2 (19 units) and Block 3 (9 units) would be located to the southern end of the site adjacent to the railway line to the east. To the west of the site, and block 2 and 3 are 1-15 Brighton Terrace and residential properties which front on to Woodlands Avenue.
- 4.3 Block 1 and 2 would be the tallest at four storeys. Block 3 and 4 would be three storey buildings. As demonstrated in the comparison plans provided the heights of the buildings are commensurate with the previously approved scheme.

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- 4.4 Each block would have its own shared outdoor amenity space and the majority of units would have private balconies. 53 parking spaces are provided within the site.
- 4.5 The key changes from the 2018 scheme can be summarised as follows:
- Internal reconfiguration of Blocks 1, 2 and 3 to provide an additional 8 apartments. All apartment types and layouts reviewed to provide more efficient buildings and better living accommodation.
  - External re-design to all blocks to: Amend the design, appearance and articulation of all blocks. Addition of balconies to the majority of apartments at first floor and above in order to provide private residential amenity space
  - Minor amendments to the site layout to: Provide 6 additional parking spaces (53 total, 47 previously approved). Ensure unobstructed access to the railway line (as required by Network Rail) for maintenance and emergency purposes
- 4.6 The proposals continue to allow for the retention of the historic Goods Station Eastern Elevation into the elevation of Block 2, and the retention and restoration of the Victorian stable block to provide bicycle and bin storage space as was previously approved. Details of the methodology for restoration of each element have previously been approved as part of discharge of pre-commencement conditions and the same details are submitted with this application.
- 4.7 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.8 Evidence of the applicant's design approach is set out below:

Assessment	The D&A Statement states that the site is on the southern side of Hooley Lane, opposite the junction with Brook Road. The railway line bounds the site to the east and to the west the site adjoins Brighton Terrace and the rear garden boundaries of properties on Woodlands Avenue. Opposite the site on Hooley Lane is Niche Place, a three-storey apartment block also occupying an elevated position at the corner of Brook Road and Hooley Lane.
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	<p>Also opposite on Hooley Lane is the Marquis of Granby public house, and the more recent development at Artillery Place.</p> <p>The Statement details the local character and context at paragraphs 2.8-2.13. The summary states that the character of the area is defined quite strongly by the influences of Victorian era development; however, there are examples of more recent infill development as well as larger scale and more dominant blocks associated with Niche Place and Tilehurst Drive. Key characteristics of the area are buildings generally fronting quite close to the road frontage with little set back and the predominance of brick as the main facing material, mostly red but with some lighter orange and yellow stock.</p> <p>The Victorian stable block is proposed to be retained and converted. The Eastern Elevation of the historic Goods Station shall be included into the elevation of Block 2</p>
Involvement	No details of community consultation are provided.
Evaluation	<p>The statement says that these revised proposals have been carefully designed to broadly adhere to the principles of layout, scale, height and design which were accepted in the previous approval whilst making better use of the site – reflecting local and national policy imperatives – to provide housing and secure a deliverable scheme.</p> <p>A full review of the consented scheme was undertaken as a direct response to the viability pressures on the scheme, together with emerging buyer demands/requirements in light of the global pandemic, particularly in relation to working from home and access to private amenity space. This revised application stems from that review process.</p> <p>Other options were explored to enhance the viability of the scheme and deliver the improvements required (including additional floors to Block 1 and repurposing of the undercroft parking to create additional units); however, these were dismissed in response to concerns raised by the Council during pre-application discussions.</p>
Design	The Statement explains that whilst following the key parameters of height, scale and massing and respecting the overall design ethos of the approved scheme, the current proposals are a positive evolution in terms of architectural quality and design merit. The elevations display greater articulation and attention to detail and the

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	scheme would make a significantly more positive contribution to the Hooley Lane street scene. The use of subtly different brick colours, bonds and brick details within the scheme reflects the character of the area.
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## 4.9 Further details of the development are as follows:

Site area	0.52ha
Existing use	Mixed commercial/industrial
Proposed use	Residential (flats)
Proposed parking spaces	53
Parking standard	82 (minimum)
Number of affordable units	4 (6%) (2018 scheme 5%)
Net increase in dwellings	68
Proposed site density	131 dph (2018 scheme - 115)
Density of the surrounding area	Varied 62dph – Brighton Terrace 145dph – Artillery Court 141dph – Niche Place (Brook Road)

## 5.0 Policy Context

### 5.1 Designation

Urban Area  
Locally Listed Building (railway warehouse)  
Parking Standards score – medium

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS4 (Valued Townscapes and Historic Environment)  
CS5 (Valued People/Economic Development),  
CS8 (Area 2a:Redhill),  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),  
CS12 (Infrastructure Delivery),  
CS14 (Housing Needs)  
CS17 (Travel Options and accessibility)

### 5.3 Reigate & Banstead Development Management Plan 2019

EMP4 (Safeguarding employment land and premises)

DES1 (Design of new development)  
DES4 (Housing mix)  
DES5 (Delivering high quality homes)  
DES6 (Affordable Housing)  
DES7 (Specialist Accommodation)  
DES8 (Construction Management)  
DES9 (Pollution and contamination land)  
TAP1 (Access, Parking and Servicing)  
CCF1 (Climate Change Mitigation)  
CCF2 (Flood Risk)  
INF3 (Electronic communication networks)  
NHE2 (Protecting and Enhancing Biodiversity)  
NHE3 (Protecting trees, woodland areas and natural habitats)  
NHE9 (Heritage assets)

#### 5.4 Other Material Considerations

National Planning Policy Framework  
2021 (NPPF)

National Planning Practice Guidance  
(NPPG)

Supplementary Planning  
Guidance/Documents

Surrey Design  
Local Character and Distinctiveness  
Design Guide SPD 2021  
Climate Change and Sustainable  
Construction SPD 2021  
Vehicle and Cycle Parking  
Guidance 2018  
Affordable Housing

Other

Human Rights Act 1998  
Community Infrastructure Levy  
Regulations 2010

#### 6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms. Appropriate residential growth is actively encouraged by the Core Strategy, in line with the "urban areas first" approach in Policy CS6.

6.2 The proposal would result in the loss of employment space. However, the loss of the employment space has already been found to be acceptable under the previously approved schemes and therefore the redevelopment of this site for residential use has already been accepted by the Council. There is therefore no in principle objection to the proposals.

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6.3 The main issues to consider are:

- Design appraisal and impact on heritage assets
- Housing Mix, Affordable Housing and Standard of Accommodation
- Neighbour amenity
- Highway matters
- Sustainable construction
- Flooding and Drainage
- Contamination
- Ecology and trees
- Crime?
- Community Infrastructure Levy

## Design appraisal and impact on heritage assets

- 6.4 As set out above the proposals would be broadly similar in nature, access, layout, scale and design of buildings to the previously approved scheme (18/00967/OUT). From a scale point of view whilst the current proposed would have 8 extra units when compared to the 2018 scheme the four blocks would be the same number of storeys as previously approved (Block 1 and 2 being 4 storeys and block 3 and 4 being 3 storeys and the overall footprint of the buildings would actually be marginally smaller than the approved scheme. The only block which has more bulk added to it would be block two however this would represent a relatively small increase and the top floor would remain set back and recessed to reduce the bulk of the building at the top level. It is therefore considered that the height, scale and massing of the proposed development is commensurate with the approved 2018 scheme and would not be obtrusive or out of keeping with their surroundings.
- 6.5 In terms of the design of the proposed blocks it is considered that the proposal represents an improvement to the approved scheme. The inclusion of balconies and required re-design has resulted in the blocks all having more articulation and interest which has helped to break up the larger elevations proposed under the 2018 scheme. This is most apparent on the submitted comparison plans which show Block 1 and 4 fronting on to Hooley Lane. Block 4 has now been reconfigured so that the elevation facing Hooley Lane is better articulated and active creating a better relationship with the street scene and Block 1. Block 1 has a lighter penthouse level helping to mitigate against the mass of the building and has a lower ridge height at the western end which helps to step the height of the building better down to Block 4.
- 6.6 In terms of materials render has been removed from the blocks, which is considered a positive as such a material can age badly in a such a location next to a busy road and railway line. Instead brick facing is proposed which better reflects the prevailing character of the area. There will be variation in brick colour, bond and arrangements to add interest. Cladding is proposed to the penthouse levels on blocks 1 and 2 to provide contrast and further break up the mass of the buildings. Such an approach is considered acceptable

and a condition is recommended to secure further details of the proposed external materials.

- 6.7 In terms of the internal layout. The additional 8 units has resulted in an increase in parking spaces and therefore hardstanding. However this is considered to have been done relatively well and overall the layout still retains adequate outdoor amenity space and space for soft landscaping and trees which will help to soften the development and ensure it does not have an unduly urbanised appearance.
- 6.8 With regard to heritage considerations this was looked at in detail when the 2018 scheme, and previous appeal scheme, were determined. Three buildings were considered to have some heritage significance – The Goods Station shed, railway warehouse and stable block. As with previous applications, it is proposed to demolition the railway warehouse at the southern end of the site. This was considered acceptable due to the fact that this building was not part of the original station, as originally thought when it was listed. It is proposed to retain the most significant element of the Goods Station shed and incorporate in to Block 2 in accordance with the details already approved under discharged of condition for the 2018 scheme (18/00967/DET05). The Victorian Stable block is proposed to be retained as an ancillary communal building in accordance with the same details approved under the 2018 scheme (18/00967/DET12). The Conservation Officer is content with this approach and subject to conditions securing this raising no objection to the proposal.
- 6.9 It was also concluded that the impact on the setting of the listed building on the opposite side of Hooley Lane (10 Hooley Lane) would be neutral or slight positive. Due to the similar scale of the proposed buildings and the improvement to the design of the blocks it is considered that there would not be any material harm to the setting of the listed building.
- 6.10 Overall, whilst the scheme has changed compared to the 2018 scheme, it is considered that the proposals still achieve a good standard of design and a development, in keeping with the surroundings. In this regard, whilst it would increase the density of the site and thus maximise its capacity for development, it would do so without material harm or detriment to character of the area or result in unacceptable harm to the identified heritage assets. Conditions are recommended to secure details of external materials, landscaping, boundary treatments, and the works to the heritage assets.

#### Housing Mix, Affordable Housing and Standard of Accommodation

- 6.11 The proposed mix would be 26x1bed flats and 38x 2 bed market units and 4x1bed affordable units.
- 6.12 In terms of market housing Policy DES4 states that on sites of 20 homes or more, at least 30% of market housing should be provided as smaller (one and two bedroom) homes and at least 30% of market housing must be larger (three+ bedroom) homes. In this case the proposal would provide no larger

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homes. Policy DES4 states that this is acceptable as long as “it can be demonstrated that it is not financially viable or technically feasible to do so, that there would be no need or market demand for a particular size of homes (as may be the case for certain types of specialist accommodation), or that doing so would have an adverse impact on the character of the surrounding area.”

- 6.13 In support the applicant has provided the following:
- Firstly, the existing approved scheme (18/00967/OUT) which represents a fallback position, provides solely 1- and 2-bedroom units, and does not provide any 3+ bedroom units;
  - There is little, if any, demand for 3-bedroom apartments in this location in Redhill. The location and characteristics of the site are such that it is suited to smaller households, particularly first-time buyers and young professional/commuter households, which simply are not in the market for large 3-bedroom family sized flats. Advice from local agents is that, in contrast to London and Outer London areas where demand dynamics are different; households looking for a 3-bedroom/family sized property in this area are looking for houses not flats. As such, there would be no demand for this type/size of unit; and
  - Allied to point 2 above, to provide 30% would therefore be unviable. Pricing of 3-bedroom units would have to reflect low levels of demand and therefore would be artificially low when compared to 1- and 2-bedroom apartments.
  - Providing 3-bedroom units would also significantly reduce the overall number of homes which can be provided on the site, and therefore the overall contribution to housing needs and supply.
- 6.14 Taking in to account the above factors officers consider that the absence of 3+ bedrooms is justified in this case.
- 6.15 In terms of affordable housing the application proposes to provide 4 shared ownership units. The mix would be 4 x 1bed 2person flats. This is well below the 20 units required by policy DES6 of the DMP but is 1 more unit than was proposed and agreed under the approved 2018 scheme. The 2018 scheme went through a thorough viability appraisal where it was agreed that only 3 units was viable. As part of the agreement for 3 units a claw back mechanism was agreed whereby the Council would receive 50% if any uplift in the Gross Development Value as a further contribution towards affordable housing. The applicant has stated that they are willing to agree to such a claw back mechanism again. The applicant has stated that the increase in 8 units from the original scheme is to bring the scheme to the point where it is commercially viable so it can be built out. The offer of 1 additional unit is given on the basis that the applicant is not required to go through the full viability assessment all over again. Officers have therefore sought further clarification regarding the viability issues of the scheme. This was provided via a letter dated 12/9/21 it further sets out the background to the site and previous schemes and then sets out the additional cost pressure facing the development site. There are four areas where unforeseen site specific costs has affected viability:

- Heritage – the works to conserve the historic buildings on site is significantly higher than expected and was not accounted for in the previous schemes abnormal costs allowance
  - Network Rail - The costs associated with securing the necessary permissions, licences, applications and legal processes required by Network Rail to retain access to the railway are considerable, between £137,500 to £287,500
  - Utilities – additional unforeseen costs in relation to mains water supply amounting to £160,000.
  - Civil Engineering – provision of new, extended and replacement retaining walls has been costed on a preliminary basis at £160,000
- 6.16 In addition the applicant has noted that the wider construction industry is currently experiencing significant rises in construction costs. The applicant also noted that the addition of balconies, which is now considered essential given buyer expectations for better private amenity space, has increased build costs by approximately £300,000.
- 6.17 The letter also points out that the clawback mechanism would provide the Council protection that, should the changes now proposed result in an unanticipated improvement in the end value of the scheme, the Council would also share in that uplift through additional monetary contributions to affordable housing. It is also stated that the increase in affordable units (25%) represents a proportionately greater increase than is proposed for market dwellings (12%). The applicant notes that this pro-rated approach in lieu of a full viability re-assessment was accepted on the scheme at Brook Road Garage (19/00210/OUT).
- 6.18 Following consideration of the above factors officers are satisfied that it is extremely unlikely that, were the applicant to go through the full viability assessment, the scheme would be able to provide more than the 4 affordable units on offer. Indeed it appears likely that the viability process would show that actually less than 4 units would be viable on this site. The Housing Officer has advised that the tenure type and mix would be acceptable. It is therefore concluded that the level of affordable housing provided, combined with the claw-back mechanism, is acceptable in this case.
- 6.19 Policy DES5 requires that all new residential development must provide high quality adaptable accommodation and provide good living conditions for future occupants. New accommodation must meet the relevant nationally prescribed internal space standard for each individual unit unless the council considers that an exception should be made. Sufficient space must be included for storage, clothes drying and the provision of waste and recycling bins in the home. Adequate outdoor amenity space including balconies and terraces and /or communal outdoor space should be provided.
- 6.20 The drawings submitted demonstrate that each flat proposed would accord with the appropriate space standard and each unit is provided with sufficient storage space. Private amenity space is provided in the form of external balconies or terrace areas for the majority of the units. The units without

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balconies are the smaller units, unlikely to have families occupying them and these units have been provided with Juliet balconies. Each block has an area of outdoor amenity space, which would not be significantly different from the approved applications. The majority of the flat's living room/kitchen areas would be dual aspect and overall it is considered that the flats would have adequate outlook and light.

- 6.21 Given the proximity of the site to the railway line, the applicant has provided noise and vibration assessments in order to evidence that a satisfactory residential environment could be achieved for future occupants. These are the same reports that were previously considered acceptable by the Council under the previous applications.
- 6.22 In relation to vibration, the assessment by NVE concludes that Vibration Dose Values which would be experienced by occupants would fall within the range where it is considered there would be "low probability of adverse comment" by residential occupants (BS6472-1). These findings are not disputed and, subject to the structural recommendations in the report, it is concluded that vibrations levels would not be so severe as to cause an unacceptable amenity for future occupants.
- 6.23 In respect of noise, the assessment by Martec concludes that, subject to achieving uprated specifications on windows to habitable rooms and the use of appropriate ventilation systems (for which specific recommendations are made), the development would achieve acceptable internal noise levels meeting the BS8233:2014 design targets. This could be secured by condition and subject to this the development would achieve an acceptable noise environment.
- 6.24 It is also noted that the site, due to its size, and parking areas are likely to require some form of external lighting. In order to prevent unacceptable light levels to both the future occupants and neighbouring properties a condition is recommended to secure further details of any external lighting prior to installation.
- 6.25 It is therefore considered that the scheme would provide good living conditions for future occupants and would comply with the requirements of DMP Policy DES5.
- 6.26 Policy DES7 of the DMP requires that on sites of 5 or more homes at least 20% of homes should meet the Building Regulations requirements for 'accessible and adaptable dwellings' and that on sites of 25 or more homes, at least 4% of homes should be designed to be adaptable for wheelchair users in accordance with the Building Regulations requirements for 'wheelchair user dwellings'. The applicant has not referred to this requirement. Without any evidence to the contrary it is considered that such a requirement would be viable for the applicant and therefore a condition is recommended to secure adequate accessible housing in accordance with policy DES7.

## Neighbour amenity

- 6.27 The site directly adjoins a number of existing dwellings on Woodlands Avenue and Brighton Terrace and there are other neighbours opposite the site on Hooley Lane. The impact of the proposal on these neighbours has been carefully considered.
- 6.28 Blocks 1 and 2, given their positioning, scale and separation to neighbouring properties, are not considered to give rise to unacceptable effects on neighbour amenity. The overall height of Block 2 would not be that dissimilar to the existing approved scheme and it would be sufficiently far from the side/rear boundaries of properties on Woodlands Avenue and Brighton Terrace (approximately 12-13m) so as to not cause an unacceptable overbearing or overshadowing effect. Given the distances and changes in levels (land to west of site at a higher level), it is also concluded that the front facing windows on Block 2 would not cause unacceptable loss of privacy for these existing neighbouring properties. The nearest existing properties to Block 1 would be on the opposite side of Hooley Lane, due north of the application site and Block 1. Given their positioning, orientation and juxtaposition compared to Block 1, it is not felt that they would experience a serious loss of amenity albeit there would be some change.
- 6.29 Blocks 3 and 4 are considered to be most sensitive in terms of their impact on neighbour amenity, Block 3 in respect of its relationship to properties on Woodlands Avenue and Block 4 due to its impact on Brighton Terrace.
- 6.30 Block 4 would be sited due north of Brighton Terrace which adjoins the site. Brighton Terrace is elevated above the application site, as shown on the survey plans, by a considerable amount in places. Whilst the neighbours at Brighton Terrace would experience a change in relationship and outlook as a result of the construction of Block 4, the proposed building in this case is largely unchanged compared to the approved scheme in terms of its siting, height, scale and massing. Block 4 was not previously identified by either the Council or the Inspector (in the 2017 appeal) as causing harm to amenity and there have been no other changes which would warrant taking a different view in this case with regard to overbearing impact and loss of light. In terms of loss of privacy the elevations are different to the previously approved scheme. The layout has been re-organised so that the only windows facing Brighton Terrace serve the stairwell. These can be conditioned to be obscure glazed and restricted opening to prevent loss of privacy. There are balconies proposed however these would be on the west and north elevations. The closest balconies would be on the west elevation, approximately 10 metres to the boundary and approximately 25 metres from the rear elevations of Brighton Terrace. Taking in to account the increased ground levels of Brighton Terrace, the separation distance and the side to rear relationship it is considered that the balconies would not result in unacceptable overlooking and loss of privacy in this urban context
- 6.31 Turning to Block 3, again this block would be very similar to the approved scheme in respect of both its siting, footprint and massing. Therefore the

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impact of the approved block, and mindful of the significant difference in levels, would not be such that it would cause an unacceptable overbearing or overshadowing effect on the adjacent property to the west.

- 6.32 The revised design and layout of fenestration and balconies also requires consideration in terms of potential for overlooking and loss of privacy. The proposed building would have additional side facing windows at first floor and above (when compared to the extant scheme) in the flank of the building facing towards no.12 Woodlands Avenue. However, these either serve bathrooms, communal corridors/stairwells, or are secondary windows to habitable rooms (lounge/kitchen); as such, it would be reasonable for these to be obscure glazed and restricted opening to safeguard against loss of privacy for the neighbour. Submitted plans have been provided showing this and a condition can be recommended to secure the finalised details of the restricted opening. There would be one clear glazed west facing window but this would be on the ground floor and would therefore have restricted outlook to the neighbour due to the change in levels (being much lower level) and any subsequent boundary treatment/retaining wall. There are balconies proposed on the north elevation. These would be approximately 9metres from the boundary. Their location would not be dissimilar to the proposed windows on the previously approved scheme, therefore subject to a condition requiring details of a side screen on these balconies to prevent sideways views directly towards no.12 it is considered that the views allows from these balconies would be so oblique that they would not cause unacceptable harm.
- 6.33 Taking the above into account, whilst neighbouring properties would experience some change as a result of the development, the proposals would not give rise to a serious detriment to their living conditions and thus comply with policy DES1 of the DMP and the general provisions of the NPPF (para 127) which seeks to ensure that developments provide a high standard of amenity for existing and future occupants.
- 6.34 The site is located within close proximity of a number of residential properties. To reduce the impact on neighbouring residents were the application to be approved a condition is recommended to secure the submission of a Construction Management Statement which addresses matters such a working hours and potential disruption from noise and pollution.

## Highway matters

- 6.35 The application proposes to maintain the access from Hooley Lane with a new access road created stretching into the site.
- 6.36 With regard to highway safety The County Highway Authority has considered the proposed access arrangement and has advised that there is no highway safety issue.
- 6.37 With regard to pedestrian access to the site, the CHA notes that there is no footway on the southern side of Hooley Lane at present and, as such, pedestrians would have to cross the one way road to gain access to the

footway on the northern side which is not ideal from a highway safety perspective particularly given the restricted visibility caused by parked vehicles on the south side of Hooley Lane. As a result, the CHA has recommended provision of an uncontrolled crossing point adjacent to the site access to provide a safe place for pedestrians to wait before crossing the road and would have the added benefit of increasing visibility of oncoming vehicles by allowing pedestrians to see past the parked vehicles. A condition is recommended to secure the implementation of the crossing prior to the occupation of the site.

- 6.38 It is also noted that concerns have been raised regarding the existing pedestrian access which the properties of Brighton Terrace benefit from. The applicant has confirmed that the access would be retained. The submitted plans show that the existing pathway would link in to the paths within the application site to allow a route to Hooley Lane.
- 6.39 In terms of refuse the access and strategy is the same as the previously approved scheme and the layout is very similar to the approved scheme. Tracking diagrams have been provided which demonstrate that a refuse freighter could manoeuvre within the site and enter and exit in forward gear. A condition is recommended to secure further details of the refuse collection strategy including details of the bin store capacities and collection points.
- 6.40 In terms of parking Policy TAP1 of the DMP states that all types of development should include car parking and cycle storage for residential and non-residential development in accordance with adopted local standards (see Annex 4) unless satisfactory evidence is provided to demonstrate that non-compliance would not result in unacceptable harm. Such evidence could include on-street parking surveys, evidence of parking demand, and/ or further information on accessibility. Development should not result in unacceptable levels of on-street parking demand in existing or new streets. Annex 4, under the Residential Standards Section p.174, does state that "The standards are provided as a guide and they may be varied at the discretion of the Council to take into account specific local circumstances" and that "A lower amount of parking may be appropriate in areas within, or adjacent to town centres."
- 6.41 In this case a total of 53 parking spaces are proposed within the site, equivalent to 0.78 per unit. The proposed level of parking would therefore fall short of the minimum requirements of the DMP.
- 6.42 A number of considerations have been put forward to justify the under provision which can be summarised as follows:
- Comparison to the approved 2018 scheme: Following the increase in the number of parking spaces proposed to 53. The number of parking spaces per unit of 0.78 would be identical to the 2018 scheme. Further due to the change in the mix of the units the number of spaces per occupant would increase from 0.24 (47 spaces for max 195 occupants) to 0.28 (53 spaces for max 189 occupants). The parking

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would therefore be no worse than the previous permission and would in fact be slightly better in terms of spaces per occupant. The extant permission is a material consideration which must be taken into account when considering this application.

- The submitted Transport Technical Note (by Motion Consultants) provides further analysis of car ownership data from the 2011 Census and within the geographic areas around the site which shows average car ownership of between 0.56 and 0.84 cars per unit for flatted schemes as well as information regarding a recent permission in Brook Road (19/00210/OUT) which had a ratio of 0.65 spaces per unit. This further demonstrates that the proposed level of car parking is justified by local car ownership characteristics and recently approved schemes.
- The location of the site is considered sustainable, close to the Brighton Road Local Centre which has a number of retail and commercial units and within walking distance of Redhill Town Centre and Redhill Train Station and several bus stops which serve the surrounding area.

6.43 The CHA has also raised no objection to the shortfall in parking due to the existing parking restrictions in the area which would prevent inappropriate on street parking.

6.44 On this basis, it is concluded that non-compliance with parking standards would not result in unacceptable harm over and above the previous extant consent and no objections are raised with regards to the under provision of parking on the site. Conditions are recommended to secure the provision of the agreed car and cycle parking provision. A condition is also recommended to secure electric charging points, Travel Statement and Construction Transport Management Plan.

6.45 Therefore, subject to the conditions recommended by the Highway Authority and a condition to secure adequate refuse provision, the proposal is considered to be acceptable in transport, parking and highway terms and thus complies with policy DES1 and TAP1 of the DMP.

## Sustainable construction

6.46 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations.

6.47 The application includes an Environmental performance Statement. This sets out that the development would achieve a 19 improvement in DER over TER through energy saving measures, façade energy efficiency and the installation of 240m<sup>2</sup> of PV panels. The report also states that the Water consumption would be limited to 105 l/p/d through the specification of flow restrictors on taps, shower and dual flush toilets.

- 6.48 In the event that planning permission is to be granted, a condition could be imposed to secure the implementation of the recommended measures in order to comply with DMP Policy CCF1.
- 6.49 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP. As above a condition is also recommended to secure the implementation of electric car charging points throughout the site.

### Flooding and Drainage matters

- 6.50 The site is in Flood Zone 1 and is therefore at a low risk of fluvial flooding and does not require a site specific Flood Risk Assessment. The application is however, given its size, required to incorporate sustainable drainage systems. In this respect, the application was supported by a drainage strategy.
- 6.51 This strategy has been considered by Surrey County Council as the Lead Local Flood Authority who has concluded that it meets the requirements of national technical standards. They therefore raise no objection subject to a condition securing finalised details of the drainage strategy and implementation.

### Contamination

- 6.52 The Council's Environmental Protection Officer has, under the previous applications, identified the potential for ground contamination to be present on and/or in close proximity to the application site. The applicant has submitted a phase 1 and phase 2 report with the application.
- 6.53 Both reports have previously been considered by the Council and found acceptable through the discharge of condition process for the 2018 permission (the Phase 1 report under 18/00697/OUT and the Phase 2 report as part of 18/00697/DET08 & DET09)
- 6.54 The Phase 2 report found some evidence of arsenic and lead in some areas and remediation is recommended. The necessary remediation identified within the report can be secured by appropriately worded planning conditions requiring submission and implementation of a Remediation Method Statement, and submission of a pre-occupation verification report.

### Ecology and Trees

- 6.55 The original ecology report for the approved 2018 scheme was submitted with the application. This report identified that, whilst there are some habitats on site, there have low ecological value and the proposal would not adversely affect the overall ecology of the site. The potential for vegetation around the site to support breeding birds is identified and the report contains recommendations as to construction practices and habitat mitigation to ensure there would not be an adverse effect. The site is concluded as having

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low potential for bat roosting; however, given the mobility of bats, recommendations are made as to precautionary practices and habitat enhancement. This report was found to be acceptable under the 2018 application. However due to the age of the report, 29 March 2018, an updated Preliminary Ecological Assessment (PEA) & Bat Survey Report has been submitted during the application process to ensure that the potential impact to habitat and protected species can be fully considered.

- 6.56 The updated PEA report advises that there would still be no significant likely impact on any Local Nature Reserves or statutory designated sites. The habitats within the site have not significantly changed and still comprise of buildings and hardstanding. Therefore no habitats of value will be impacted by the proposed development. In terms of protected species there remains no evidence of or suitable habitat for badgers, Great Crested Newts and other Amphibians, reptiles or any other notable species such as brown hare, harvest mouse, hedgehog, invertebrates or plants. Like the 2018 report the site has habitat suitable for breeding birds and mitigation requirements.
- 6.57 With regard to bat survey bats the majority of the buildings are still not considered suitable for bat roosting and no evidence of roosting bats. The exception is part of building 4. This
- 6.58 The report also sets out a number of biodiversity measures which would enhance the biodiversity throughout the site including bat and bird boxes, landscaping and sensitive lighting.
- 6.59 The findings of this report are agreed and subject to a condition requiring adherence to the mitigation measures recommended and a condition securing further details of the enhancement measures it is considered that the scheme would comply with policy NHE2 of the DMP.
- 6.60 In terms of the impact on trees an arboricultural survey and impacts assessment has been undertaken and this has been submitted to accompany the application. This demonstrates that most trees on and around the site are of comparatively low value (Grade C or Grade U). It does however note a small number of higher grade (Grade B) trees (Lime and Sycamore) close to Hooley Lane and Brighton Terrace. 3.42 These higher value trees can be retained and the arboricultural impact assessment demonstrates that the development can be built without undue impact to the health and longevity of the trees. The trees on the northern boundary at the front of the site provide particularly good screening of the site when viewed from the green space on Woodlands Road.
- 6.61 The arboricultural information submitted with the application has been reviewed by the Council's Tree Officer who has advised the following:  
"My comments are based on a desktop assessment of the arboricultural report by Broad Oak Tree Consultants reference J49.20 dated 5th April 2018. The majority of the existing tree stock has been classed a low quality and their removal will have limited impact on the appearance of the area. The line of mature limes (T1-T5) is shown to be retained and will form a mature

screen for block 4. It is likely that they will need to be pruned from time to time to alleviate issues such as cutting back parts of the canopy which extend over the block. The proposed layout allows a landscape scheme which includes adequate space for replacement trees to be planted and enhance the appearance of the site. Based on the existing arboricultural report, I support this application subject to the following conditions being attached to the decision notice.”

- 6.62 It should also be noted that there has been no previous objection to redevelopment of the site on arboricultural grounds from the Council's Tree Officer and the current application does not substantively change the layout, position of buildings or relationship to trees compared to that previously approved.
- 6.63 Therefore, whilst there would be some tree losses, subject to conditions to secure tree protection and soft landscaping details, the arboricultural impacts of the development are not considered to warrant refusal.

### Crime

- 6.64 Representations have raised general concerns in relation to crime; however, no specific issues or reasons have been identified. Policy DES1 requires that: “Creates a safe environment, incorporating measures to reduce opportunities for crime and maximising opportunities for natural surveillance of public places. Developments should incorporate measures and principles recommended by Secured by Design.”
- 6.65 The scheme is considered to be adequately designed so as to avoid undue risk or fear of crime (e.g. the main access road, amenity spaces and parking areas would all have some level of natural surveillance); no issues have been identified which would set this aside from any other residential redevelopment.
- 6.66 Surrey Police has advised that consideration should be given to the security compartmentation of block one, the access controls to be implemented in all four blocks and technical standards to doors, windows, cycle store and bins stores within the development. They recommend a condition in relation to Secure by Design to secure further details.

### Community Infrastructure Levy (CIL)

- 6.67 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and, although the exact amount would be determined and collected after the grant of planning permission.

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## Infrastructure Contributions

- 6.68 In terms of other contributions and planning obligations, The Community Infrastructure Levy (CIL) Regulations were introduced in April 2010 which state that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, affordable housing provision is required in line with the details set out in the report. No other contributions or requirements have been requested or identified. Accordingly, any request for an infrastructure contribution would be contrary to CIL Regulation 122.

## CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Proposed Plans	PL 21-572- 300	B	10.11.2021
Survey Plan	PL 21-572- 02	A	10.11.2021
Site Layout Plan	PL 21-572-04	B	11.11.2021
Proposed Plans	19-504- C12.1	B	25.05.2021
Proposed Plans	19-504- C12.2	B	25.05.2021
Proposed Plans	19-504- C5.1	B	25.05.2021
Elevation Plan	19-504- C5.2	D	25.05.2021
Site Layout Plan	ENG-DWG-VED-VC0880-001	P04	25.05.2021
Site Layout Plan	PL 21-572- 03		25.05.2021
Other Plan	PL 21-572- 202		25.05.2021
Street Scene	PL 21-572- 09		25.05.2021
Proposed Plans	PL 21-572- 400		25.05.2021
Street Scene	PL 21-572-10		25.05.2021
Other Plan	PL 21-572- 08		25.05.2021
Other Plan	PL 21-572- 07		25.05.2021
Proposed	Plans PL 21-572- 201		25.05.2021
Site Layout	Plan PL 21-572- 05		25.05.2021
Other Plan	PL 21-572- 06		25.05.2021
Other Plan	PL 21-572- 103		25.05.2021
Proposed Plans	PL 21-572- 102		25.05.2021
Floor Plan	PL 21-572- 200		25.05.2021
Floor Plan	PL 21-572- 101		25.05.2021
Floor Plan	PL 21-572- 100		25.05.2021
Street Scene	PL 21-572- 11		25.05.2021
Location Plan	PL 21-572- 01		25.05.2021

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Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan DES1.

4. No development shall commence until a Construction Transport Management Plan, to include details of:
  - (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) HGV deliveries and hours of operation
  - (f) construction vehicle routing to and from the site
  - (g) measures to prevent the deposit of materials on the highway
  - (h) on-site turning for construction vehicles

has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019, Reigate and Banstead Core Strategy 2014 Policy CS17 and Reigate and Banstead Development Management Plan September 2019 policies TAP1 and DES8

5. No development shall commence until a Construction Management Statement, to include details of:
  - a) Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours. Where potential impacts are identified, mitigation measures should be identified to address these impacts.
  - b) Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.

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c) Means of communication and liaison with neighbouring residents and businesses.

d) Hours of work.

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with Reigate and Banstead Development Management Plan 2019 policy DES8.

6. No development shall commence including demolition and or groundworks preparation until a detailed, scaled finalised Tree Protection Plan (TPP) and the related finalized Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings, type of surfacing for the entrance drive and location of site offices. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Development Management Plan 2019 policies NHE3 and DES1 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

7. No development shall take place above slab level on site until a scheme for the landscaping of the site has been submitted to and approved in writing by the LPA. Landscaping scheme shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved.

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All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and Meath Green Conservation Area, and to comply with Reigate and Banstead Borough Development Management Plan 2019 policies NHE3 and DES1, British Standards including BS8545:2014 and British Standard 5837:2012.

8. No development above ground level shall commence until a scheme to provide biodiversity benefits, informed by the submitted ecology report (The Ecology Co-op Updated Preliminary Ecological Appraisal and Bat Survey Report dated 3/9/2021), has been submitted to and approved in writing by the local planning authority (LPA). This should be designed alongside the soft landscaping proposals for the site. The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these details and before first occupation of this development unless otherwise agreed in writing by the LPA,.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and Reigate and Banstead Development Management Plan 2019 policy NHE2

9. A. Prior to commencement of the development a detailed remediation method statement shall be produced and be submitted to and approved in writing by the Local Planning Authority, in line with the recommendations of the Phase 1 Contamination Risk Assessment (ref. 11322, Sept 2014) & Phase 2 (ref. 11810-Rev.1, October 2019) Contamination Assessment prepared by Ground & Environmental Services Ltd., that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed to identified receptors at the site and details of the information to be included in a validation report and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.

B. Prior to occupation, a remediation validation report for the site shall be submitted to and approved in writing by the Local Planning. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas

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mitigation measures be required to be incorporated into a development the testing and verification of such systems should have regard to CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

10. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Development Management Plan 2019 policy DES9 and the NPPF.

11. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme has been submitted to and approved in writing by the local planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDs, NPPF and Ministerial Statement on SuDs. The required drainage details shall include:
  - a) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+40% allowance for climate change) storm events, during all stages of the development. The final solution should follow the principles set out in the approved drainage strategy. If Associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 2.63 l/s.
  - b) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.).

- c) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected from increased flood risk.
- d) Details of drainage management responsibilities and maintenance regimes for the drainage system.
- e) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

The development shall be completed in accordance with the approved details and thereafter maintained.

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with, Policy CS10 of the Core Strategy 2014, Policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

12. No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

13. No development above slab level shall take place until details setting out how the applicant will ensure that, unless otherwise agreed in writing, at least 20% of the homes meet the Building Regulations requirements for 'accessible and adaptable dwellings' and at least 4% are adaptable for wheelchair users in accordance with the Building Regulations requirements for 'wheelchair user dwellings', have been submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance with the agreed details.

Reason: In order that the scheme provides accessible housing in accordance with Reigate and Banstead Development Management Plan 2019 policy DES7

14. The development hereby approved shall not be first occupied unless and until the proposed modified vehicular access to Hooley Lane has been constructed in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other

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highway users, and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1

15. The development hereby approved shall not be first occupied unless and until an uncontrolled pedestrian crossing consisting of a build out with a dropped kerb and tactile paving has been constructed on Hooley Lane adjacent to the site access in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1

16. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking /turning areas shall be retained and maintained for their designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users and to accord with the National Planning Policy Framework and Reigate and Banstead Development Management Plan policy TAP1

17. The development hereby approved shall not be first occupied unless and until space has been laid out within the site, in accordance with details and plans to be submitted to and approved in writing, for a minimum of 68 cycles to be parked in a covered and secure location. Thereafter the cycle parking area shall be retained and maintained for its designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

18. Prior to occupation of the development a Travel Statement to include the provision of information to residents regarding the availability of and whereabouts of local public transport facilities, walking and cycling routes, shall be submitted for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework 2018 and Surrey County Council's "Travel Plans Good Practice Guide". And then the approved Travel Statement shall be implemented upon first occupation of each dwelling within the development.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

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19. Notwithstanding the drawings, the development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. Such details shall include any works, repairs or refurbishment to the existing front boundary retaining wall on Hooley Lane. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE3

20. Prior to the first occupation of the development full details (and plans where appropriate) of the waste management storage and collection points, (and pulling distances where applicable), throughout the development shall be submitted to and approved in writing by the Local Planning Authority.

All waste storage and collection points should be of an adequate size to accommodate the bins and containers required for the dwelling(s) which they are intended to serve in accordance with the Council's guidance contained within Making Space for Waste Management in New Development.

Each dwelling shall be provided with the above facilities in accordance with the approved details prior to occupation of the relevant dwellings and thereafter retained in accordance with the approved details.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage recycling in accordance with the Development Management Plan 2019 policy DES1.

21. The development hereby approved shall not be occupied unless and until a minimum of 10 of the available parking spaces are provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) and a minimum of 10 of the available spaces are provided with an electric supply to power a fast charger in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason: In order that the development promotes more sustainable forms of transport, and to preserve the character of the Conservation Area, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 and policy TAP1 and NHE9 of the Development Management Plan.

22. The development hereby permitted shall only be completed in accordance with the details of the scheme for the salvage and subsequent re-use of features of the Goods Station eastern elevation submitted with this application (including drawings 19-504-C5.1B and C5.2D).

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Reason: In order to secure a realistic strategy for the preservation of this non-designated heritage asset with regard to policy CS4 of the Reigate and Banstead Core Strategy 2014 and to Policy NHE9 of the Reigate and Banstead Development Management Plan 2019 and with regards to the provisions of the NPPF.

23. The development hereby permitted shall only be completed in accordance with the details of the restoration and conversion of the Victorian stable block submitted with this application (including drawings 19-504-C12.1B and C12.2B).

Reason: In order to secure the restoration of this non-designated heritage asset with regard to policy CS4 of the Reigate and Banstead Core Strategy 2014 and to Policy NHE9 of the Reigate and Banstead Development Management Plan 2019 and with regards to the provisions of the NPPF

24. The development hereby approved shall be carried out in accordance with the recommendations of the Railway Vibration Assessment by NVE dated 10/2/2015 ref. NVE2714-1. Any changes to the recommended methods of construction shall be submitted to and agreed in writing by the Local Planning Authority prior to the changes taking place.

Reason: To ensure that future occupants would not be exposed to unacceptable levels of vibration from the railway and in order to achieve an adequate level of residential amenity with regard to Policy DES1 and DES5 of the Reigate and Banstead Development Management Plan 2019 and policy CS10 of the Reigate and Banstead Core Strategy.

25. The development hereby approved shall be carried out in accordance with the Rail Noise Screening Assessment by Martec Environmental Consultants (dated 19<sup>th</sup> March 2015). All mitigation measures required for each unit shall be installed prior to the first occupation of that unit and retained thereafter.

Notwithstanding the approved plans and aforementioned report, details of the final siting, positioning and specification of acoustic fencing shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the first residential unit. Thereafter, the panels shall be installed prior to the occupation of the first residential units.

Reason: To ensure that future occupants would not be exposed to unacceptable levels of noise and in order to achieve an adequate level of residential amenity with regard to Policy DES1 and DES5 of the Reigate and Banstead Development Management Plan 2019 and policy CS10 of the Reigate and Banstead Core Strategy.

26. The development hereby approved shall be carried out in accordance with the Environmental Performance Statement by BRY Energy Services (dated April 2021 ref. 106-025/108-006/121-007) to ensure that the development:
- a) Restricts potential water consumption by occupants to maximum of 110 litres per person per day (report states 105 litres per day);

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- b) Achieves not less than 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations; and

All measures for each block shall be implemented, installed and operational prior to first occupation of that block.

Details of the final siting and positioning of the proposed solar photovoltaic panels shall be submitted to an approved in writing by the Local Planning Authority prior to the first occupation of the development. Thereafter, the panels shall be installed and operational on each relevant block prior to the first occupation of the that block.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

- 27. The development shall not be occupied until a scheme demonstrating compliance with the principles of 'Secured by Design' has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be completed before the occupation of the development hereby permitted and shall be permanently maintained as such thereafter.

Reason: To ensure that the development provides a secure environment for future residents in accordance with Policy DES1 of the Reigate & Banstead Development Management Plan 2019.

- 28. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
  - a) A broadband connection accessed directly from the nearest exchange or cabinet
  - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

- 29. The development shall be carried out in accordance with the mitigation measures set out within the Ecology Co-op Updated Preliminary Ecological Appraisal and Bat Survey Report dated 3/9/2021. This includes the need to secure a European Protected Species licence from Natural England.

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

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30. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls) and confirm any defects have been rectified.

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

31. No plant or machinery, including lifts, fume extraction, ventilation and air conditioning, which may be required by reason of granting this permission, shall be installed within or on the building without the prior approval in writing of the Local Planning Authority. Any approved plant or machinery shall be installed and thereafter maintained in accordance with the approved details and any manufacturer's recommendations.

Reason: To ensure that a satisfactory external appearance is achieved of the development and to safeguard the amenities of neighbouring occupiers with regard to Policy DES1 of the Reigate and Banstead Development Management Plan 2019.

32. No external lighting shall be installed on the buildings hereby approved or within the site until an external lighting scheme, which shall include indication of the location, height, direction, angle and cowling of lights, and the strength of illumination, accompanied by a light coverage diagram, has been submitted to and agreed in writing by the local planning authority.

The external lighting shall be implemented in accordance with the approved scheme and be retained thereafter and maintained in accordance with the manufacturer's instructions.

Reason: To protect the visual amenity of the area and neighbouring residential amenities with regard to Reigate and Banstead Core Strategy 2014 Policy CS10 and policy DES1, DES5 and DES9 of the Reigate and Banstead Development Management Plan 2019.

33. The first and second floor balconies hereby permitted on the northern (right side elevation on drawing PL 21-572-300 B) elevation of Block 3 shall not be used unless and until details of a privacy screen along the western side of the balconies of a height of minimum 1.7m high have been submitted to and agreed in writing by the Local Planning Authority and installed as agreed.

The privacy screen shall thereafter be permanently retained and maintained in accordance with the approved details.

Reason: To ensure that the development does not affect the amenity of existing properties by overlooking with regard to Reigate and Banstead Development Management Plan policy DES1.

34. The first and second windows on the south (flank) elevation of Block 4 shall be glazed with obscured glass and restricted opening in accordance with details to be submitted to and agreed in writing by the Local Planning Authority prior to the first occupation of Block 4. The windows shall be installed in accordance with the approved details prior to first occupation and shall be maintained as such at all times thereafter.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

35. The windows in the west (rear) elevation of Block 3 of the development hereby permitted which are annotated to be obscured and restricted opening on plan PL 21-572-300 B shall be glazed with obscured glass and restricted in accordance with details to be submitted to and agreed in writing by the Local Planning Authority prior to the first occupation of Block 3. The windows shall be installed in accordance with the approved details prior to first occupation and shall be maintained as such at all times thereafter.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. The Council's Neighbourhood Services team can be contacted on 01737 276292 or via the Council's website at <http://www.reigate->

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[banstead.gov.uk/info/20085/planning\\_applications/147/recycling\\_and\\_waste\\_developers\\_guidance](http://banstead.gov.uk/info/20085/planning_applications/147/recycling_and_waste_developers_guidance)

4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
- (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and

# Agenda Item 5

Planning Committee  
24<sup>th</sup> November 2021

Agenda Item: 5  
21/01458/F

upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigatebanstead.gov.uk/info/20277/street\\_naming\\_and\\_numbering](http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering)

7. The permission hereby granted shall not be construed as authority to carry out any works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see: [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice).
8. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority Local Highways Service Group (0300 200 1003) before any works are carried out on any footway, footpath, carriageway, or verge to form or modify a vehicle crossover or to install dropped kerbs. Please see: [www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs).
9. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
10. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

# Agenda Item 5

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24<sup>th</sup> November 2021

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11. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
12. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
13. The Travel Statement condition (no. 18) above should take the form of a 'Welcome Pack' for residents, which should include information on local bus and rail services, walking and cycling routes, and local services and facilities located in the vicinity of the site. The 'Welcome Pack' should be provided to residents on first occupation of each dwelling within the development, in order to encourage sustainable travel from the outset.
14. Condition no. 15 above therefore requires the applicant to provide an uncontrolled pedestrian crossing point adjacent to the site access, preferably on the eastern side, which should consist of a kerb build out or an area of footway/hardstanding with a dropped kerb and tactile paving. This would provide a safe place for pedestrians to wait before crossing the road and would have the added benefit of increasing visibility of oncoming vehicles by allowing pedestrians to see past the parked vehicles. A dropped kerb and tactile paving should also be provided on the opposite side of the road.
15. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
16. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. The planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Extra Heavy Standard size with initial planting heights of not less than 4m, with girth measurements at 1m above ground level in excess of 14/16cm.
17. Environmental Health would like to draw the applicant attention to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks notice'. The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even

enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.

18. Network Rail ask that the applicant/developer engages with Network Rail's Asset Protection and Optimisation (ASPRO) team via [AssetProtectionLondonSouthEast@networkrail.co.uk](mailto:AssetProtectionLondonSouthEast@networkrail.co.uk). This will allow our ASPRO team to review the details of the latest proposal to ensure that works can be completed without any risk to the operational railway. It is likely that the development will be required to enter into an Asset Protection Agreement. **Please also see the full Network Rail consultation response on the Council Website** to view a full list of their requirements with regard to developments within close proximity to the railway.

## REASON FOR PERMISSION

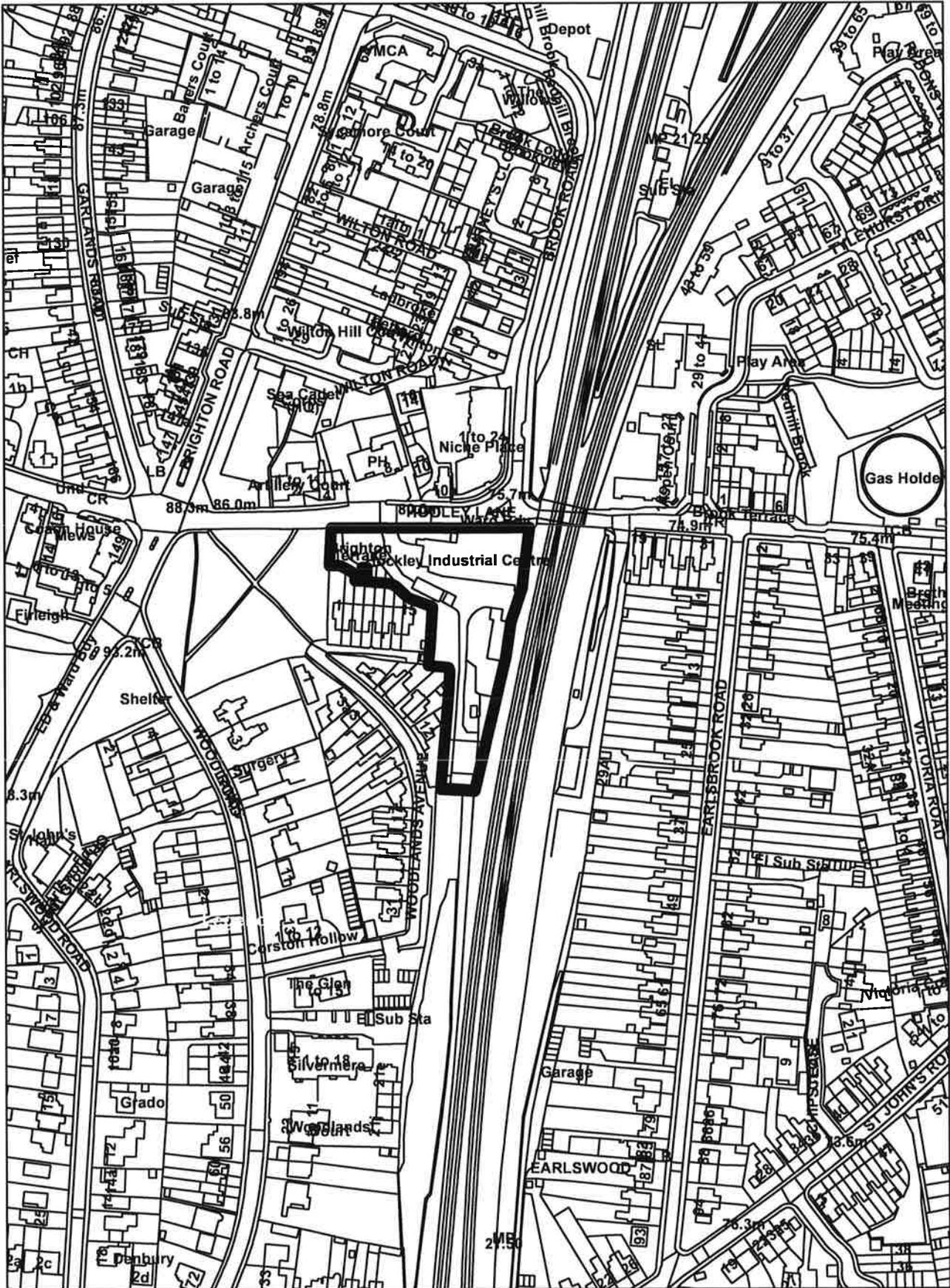
The development hereby permitted has been assessed against development plan policies CS1, CS4, CS5, CS8, CS10, CS11, CS12, CS14, CS17 and EMP4, DES1, DES4, DES5, DES6, DES7, DES8, DES9, TAP1, CCF1, CCF2, INF3, NHE2, NHE3, NHE9 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## Proactive and Positive Statements

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# Agenda Item 5

21/01458/F - Hockley Industrial Centre, Hooley Lane, Redhill



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Scale 1:2,500



# Agenda Item 5

Site: PLANNING  
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Proposed Railway Street Scene



Approved Railway Street Scene



addo  
 NORTHUS PROPERTIES LTD  
 Project: Hockley Industrial Centre  
 Railway Street  
 Surrey  
 The Railway Street Scene Comparison  
 Scale: A1 : 20 Date: 21.10.2021  
 Drawing No: PL 21-572-11 Rev: 1

This document is for planning purposes only. It is not intended to be used for any other purpose. It is not intended to be used as a contract or as a statement of fact. It is not intended to be used as a basis for any legal proceedings. It is not intended to be used as a basis for any other action.



Hopley Lane Street Scene

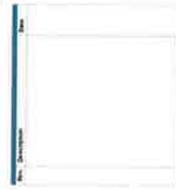


Railway Street Scene

1:500 0m 10m 20m 30m 40m 50m 60m 70m 80m 90m 100m

adda  
ARCHITECTURAL DESIGN & INTERIORS  
NAME: NORDHUS PROPERTIES LTD  
ADDRESS: Hopley Industrial Centre  
Hopley Lane  
Frosthill  
Surrey

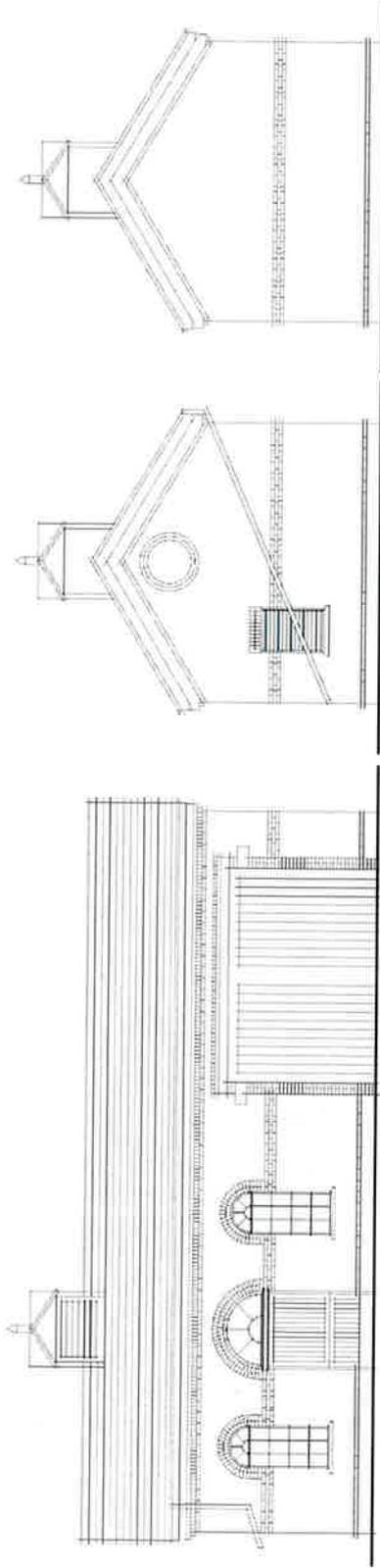
DATE: 21/04/2021  
SCALE: A1 : 1:200  
PROJECT: PL 21-572-09  
TYPE: Street Scenes



NAME: Street Scenes  
DATE: 21/04/2021  
SCALE: A1 : 1:200  
PROJECT: PL 21-572-09



18/00967/OUT - Condition 12 (Victorian Stable Block strategy - Proposed Elevations)



Victorian Stable Block Front Elevation  
1 : 100

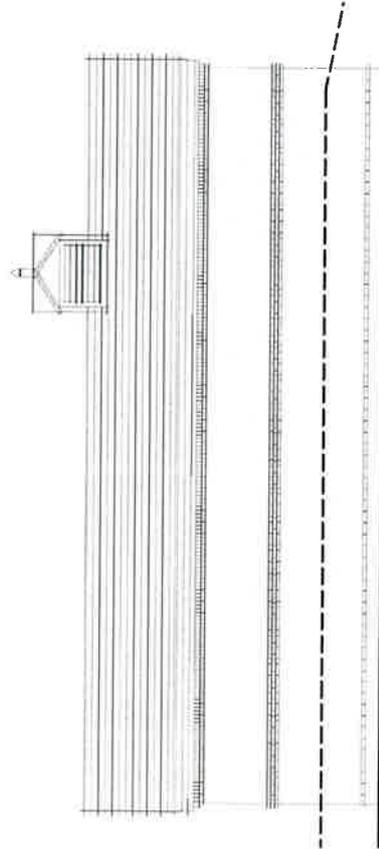
Right Elevation  
1 : 100

Left Elevation  
1 : 100

**Condition 12-**  
No works to the retained Victorian stable block shall commence until a detailed scheme for the restoration and conversion of the building has been approved by the LPA

**Note-**  
The proposal is for the refurbishment and restoration of the existing Victorian stable block to form a building for communal cycle storage and refuse.

Generally the building is to be made good and retained in its current structural form with internal adaptations to make the building suitable for its purpose. Elevationally, the intention is to keep any external changes associated with the conversion as limited as possible. Where detrimental changes have occurred in the past, these will be restored to original condition where possible and appropriate.



Rear Elevation  
1 : 100



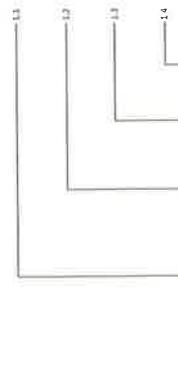
Scale: As indicated @ A3 Date: 08.06.2020

# Agenda Item 5

All brickwork in the Eastern elevation is to be taken down by hand to be re-used where possible on the new feature wall. The Western elevation will be demolished in the same manner to enable material to be salvaged. Any horizontal brickwork to the Eastern and Western elevations is to be taken down by hand for re-use, to be salvaged where possible and used with new multi-coloured bricks of colour and dimensions to match existing.

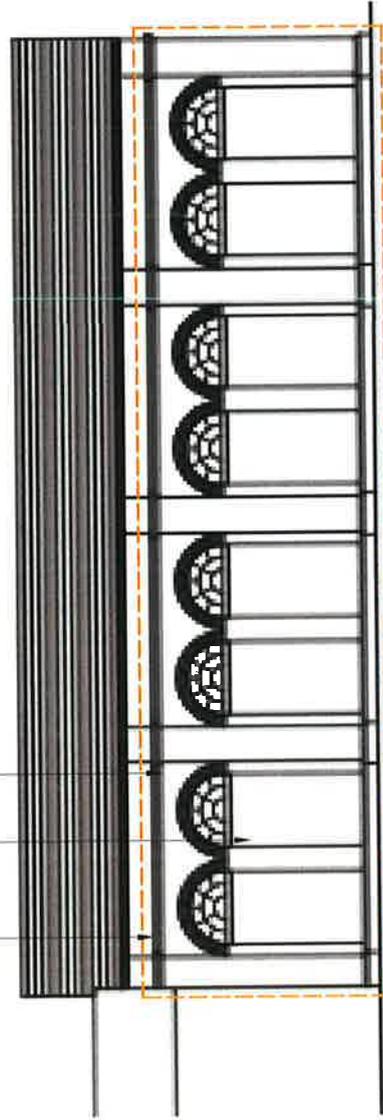
Plasters to Eastern and Western elevations are to be taken down by hand to be re-used where possible on the new feature wall. Plaster to the Eastern elevation is to be taken down by hand to be re-used where possible on the new feature wall. Plaster to the Western elevation is to be taken down by hand to be re-used where possible on the new feature wall.

Cast iron windows to be salvaged and reinstated in new feature wall. Reinstatement by cast iron specialist where necessary. Any glazing packages to be replaced with existing glass.



Existing Goods Station Eastern Elevation

Brickwork, sash and glazing packages to be salvaged from the Western elevation and stored or re-used on the Eastern elevation. Any elements from the Western elevation not to be used on the Eastern elevation can not be used.



Western Elevation

Materials salvaged to reconstruct the feature wall will be lightly cleaned of mortar, particularly the faces of the bricks, and safely stored on pallets in a locked storage container on site for re-use where required. The materials will be covered with a ply sheeting to provide additional protection from damage.

In order to ensure sufficient salvaged material, the Western elevation will also be demolished by hand so that bricks and materials can be salvaged to be re-used in the new feature wall. Any materials required for the new feature wall will be salvaged from the Western elevation and stored on site for re-use where required. A sample will be supplied to the local Planning Authority in advance of their approval.

Brick salvage and subsequent reconstruction will be undertaken with the input of a conservation brick specialist. The appointed demolition contractor will prepare a list of demolition methods statements, which will be submitted to the Conservation Officer for review.

The feature wall will be constructed as part of the works to construct Block 2, along the main structural wall. The main structural wall will be completed first. The structural wall of Block 2 will be built of cavity masonry with multi-coloured bricks or EWS, but so that the feature wall can be tied back to the new wall as it is being constructed. A vertical DPC will be incorporated between the elements to maintain water tightness.

**Condition 5-**

No development, including demolition, shall commence until a scheme for the salvage and subsequent re-use of features of the Goods Station eastern elevation has been submitted to and approved by the LPA.

**Note-**

General strategy is to provide a feature wall to the railway elevation fixed to the proposed Block 2. The features are to celebrate the existing elevation of the Goods Station Eastern side.

1.6 All existing materials to be salvaged and stored on site in a locked storage container on site for re-use where required.





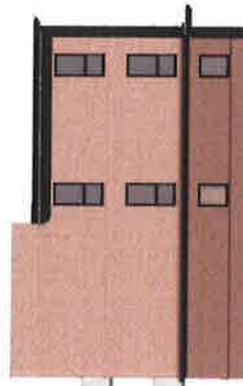
Primary Front Elevation



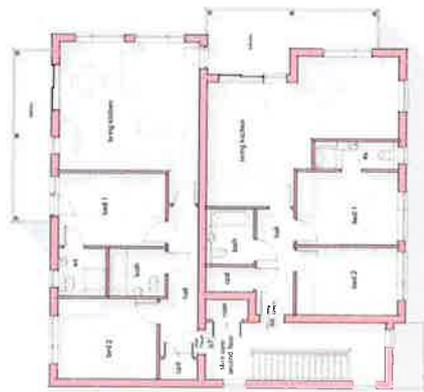
Hookey Lane Elevation



Rear Elevation



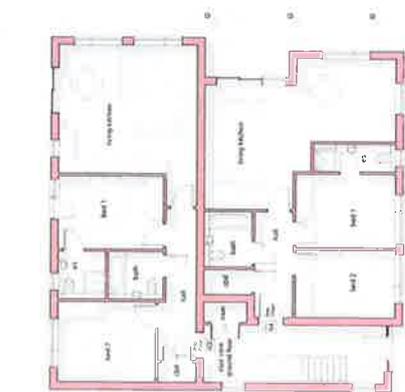
Flank Elevation to Brighton Terrace



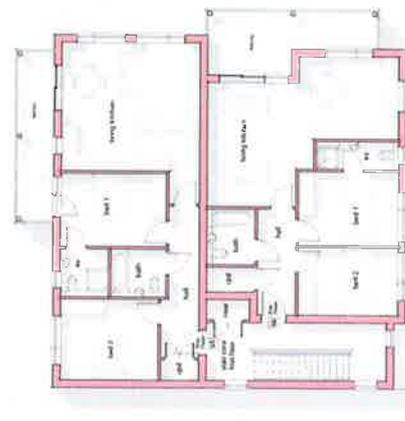
L3 Penthouse Floor Plan



Roof Plan



L1 Ground Floor



L2 First Floor



Sketch View 1 - Front Corner



Sketch View 2 - Rear Corner Fronting Hookey Lane



Sketch View 3 - Side Entrance Corner Fronting Hookey Lane



# Agenda Item 5

**addo**  
architectural design

Client: Nordhus Properties Ltd  
Project Land at: Hookey Industrial Centre  
Hookey Lane  
Hookey Lane  
Survey

Team: Nick & Plans, Elevations & Sketch  
Date: 21.04.2021  
Scale: 1:1000  
Drawing No: PL 21-572-400

# Agenda Item 5

Scale: PLANNING  
 1" = 1'-0"  
 1/4" = 3'-0"  
 1/8" = 6'-0"  
 1/16" = 12'-0"  
 1/32" = 24'-0"  
 1/64" = 48'-0"  
 1/128" = 96'-0"  
 1/256" = 192'-0"  
 1/512" = 384'-0"  
 1/1024" = 768'-0"  
 1/2048" = 1536'-0"

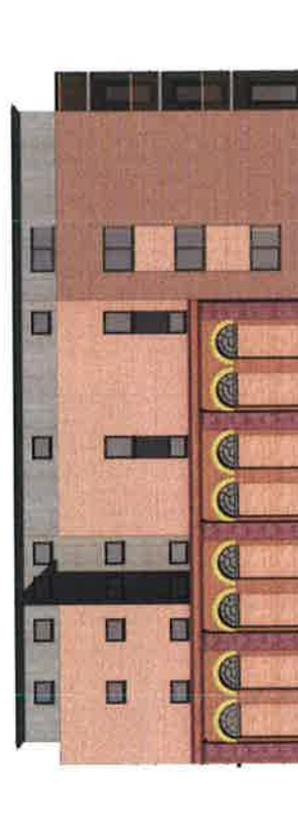
DATE: 01/11/2021



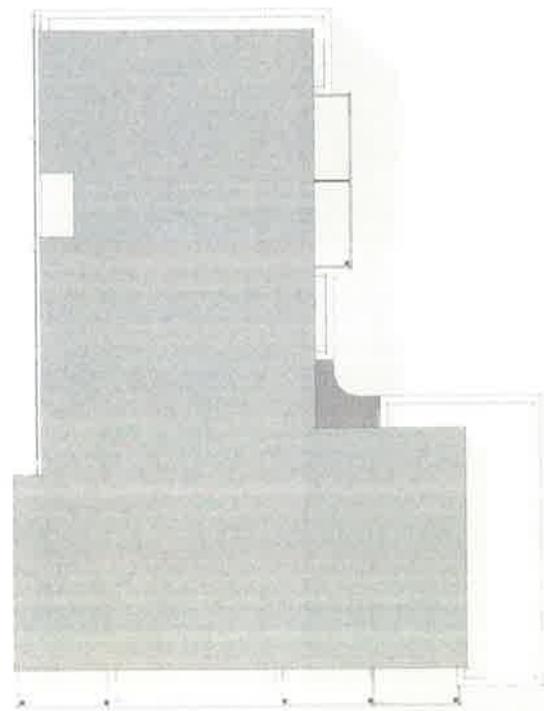
Right Flank Elevation

Front Primary Elevation

Left Flank Elevation



Rear Elevation



Floor Plan

addo  
 ARCHITECTURE  
 1000 14th Street, NW  
 Atlanta, GA 30309  
 Phone: 404.525.1100  
 Fax: 404.525.1101  
 Website: www.addo.com

Client: Nordhus Properties Ltd

Project: Landmark Industrial Centre  
 1000 14th Street, NW  
 Atlanta, GA 30309

File: Block 2-6000 Plain & Elevations

Scale: A1: 1/128" Date: 01/11/2021  
 Drawing: PL-21-572-201



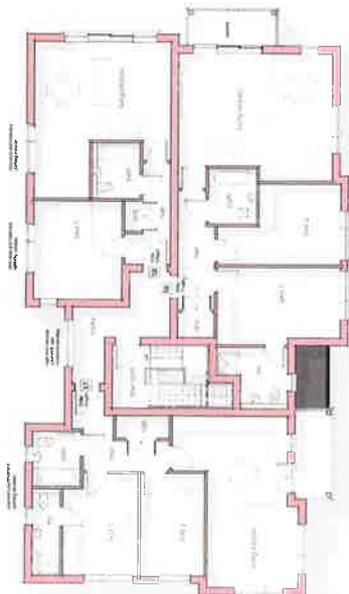
Front Elevation



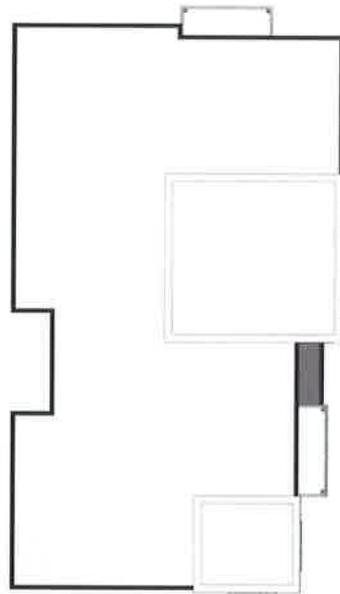
Right Side Elevation



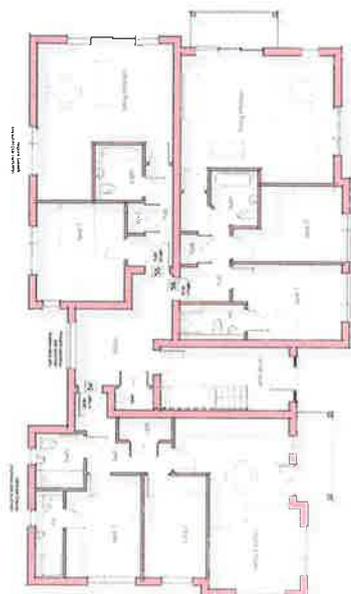
Rear Elevation



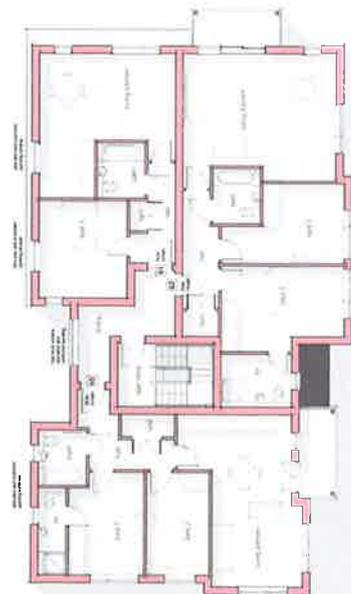
L2 First Floor Plan



Roof Plan



L1 Ground Floor Plan



L3 Penthouse Floor Plan



Left Side Elevation



Sketch View 1



Sketch View 2

# Agenda Item 5

**adda**  
drawing an experience

Client: **Northus Properties Ltd**  
Project: **Lane 26, Industrial Centre, Hookey Lane, Rochill, Surry**

Drawn: **PL 21-572-300**  
Rev: **B**

# Agenda Item 5

Source: PLANNING  
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NEW DEVELOPMENT  
 0/25



Hooley Lane Elevation



Left Flank Elevation



Sketch View 2 - Fronting Hooley Lane



Sketch View 3 - Courtyard Elevation



Sketch View 4 - Entrance Corridor Fronting Hooley Lane

**addo**  
 driving our ambition

Project: Land at Industrial Centre  
 Hooley Lane  
 Repton  
 Surrey

Client: Norchus Properties Ltd

Date: 21.06.2024

Page: PL 21-572 - 103

As shown on the site plan, the proposed development is located on the eastern side of the site, bounded by the existing industrial buildings to the north and east, and the proposed access road to the south and west.



Sketch View 1



Sketch View 2



Sketch View 3



Sketch View 4



# Agenda Item 5

adda

Client: Northus Properties Ltd  
Project: Land 4,  
Hobby Industrial Centre  
Hobby Lane  
Hobby  
Surrey

Title: Block 2-Sketch Views 1-4

Scale: A1 - Sheet: 21 of 2021  
Project: PL 21-572-202



# Agenda Item 5

Rev.	Description	Date

**Status: PLANNING**

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- Do not scale from the drawings
- The Contractor is to check all dimensions and levels before work commences
- The Contractor is to check all dimensions, levels, heights, depths, etc. against the drawings, specifications and Bill of Materials
- The Contractor is to comply with all current British Standards and Building Regulations
- Apply Clery and Addo Design Ltd of any discrepancies



Impression 1

Block 4

Entrance

Block 1

Impression 2



Block 3

Block 2

Block 1

Block 4



Addo Design Ltd  
 Company Reg No: 889627  
 10, The Quadrant  
 0: admin@addodesign  
 www.addodesign

**Client: NORDHUS PROPERTIES LTD**

**Project: Hockley Industrial Centre**  
 Hooley Lane  
 Redhill  
 Surrey

**Title: Sketch Views 1**

**Scale: A3**      **Date: 21.04.2021**  
**Drw No: PL 21-572-06**      **Rev:**

**Status: PLANNING**

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- The Contractor is to check all the dimensions and levels before work commences.
- The drawing must be read with and checked against all structural and other specialists.
- The Contractor is to comply with all current British Standards and Building Regulations
- Notes on it specifically stated on these drawings.
- Notes on it and Additions, Change, Use of any discrepancies.

**Impression 3**



Block 1

Block 2

Existing Building

**Impression 4**



Block 2

Block 3

Existing Building

**Impression 5**



Block 3

Title: Sketch Views 2

Scale: A3-

Date: 21.04.2021

Drwg No: PL 21-572-07

Rev:



Addo Design Ltd  
 Company Reg. No. 8806247  
 Addo Design Ltd  
 6, The Old Mill  
 Redhill, Surrey, GU20 0EX  
 www.addo-design.com

Client: **NORDHUS PROPERTIES LTD**

Project: **Hockley Industrial Centre**  
 Hookey Lane  
 Redhill  
 Surrey

# Agenda Item 5

**Status: PLANNING**

- Copyright: all rights have been asserted under the Design, Patents & Copyright Act 1988.
- Do not scale from the drawing.
- The Contractor is to check all file annotations and levels before work commences.
- The Contractor is to check all file annotations and levels before work commences.
- The Contractor is to check all file annotations and levels before work commences.
- The Contractor is to comply with all current British Standards and Building Regulations.
- Nelly Lakin and Addo Design Ltd of any discrepancies.

Rev.	Description	Date

Impression 6



Block 2

Impression 8



Block 2

Block 4

Block 1

Impression 7



Block 1

Block 2

**Addo Design Ltd**  
 Addo Design Ltd  
 Company Reg No: 1055298  
 U.K. VAT No: 264 247 410  
 E: [admin@addo.design](mailto:admin@addo.design)  
 W: [www.addo.design](http://www.addo.design)


  
 drawing on experience

**Client: NORDHUS PROPERTIES LTD**

**Project: Hockley Industrial Centre**  
 Hockley Lane  
 Redhill  
 Surrey

**Title: Sketch Views 3**

**Scale: A3-**

**Date: 21.04.2021**

**Dwg No: PL 21-572-08**

**Rev:**



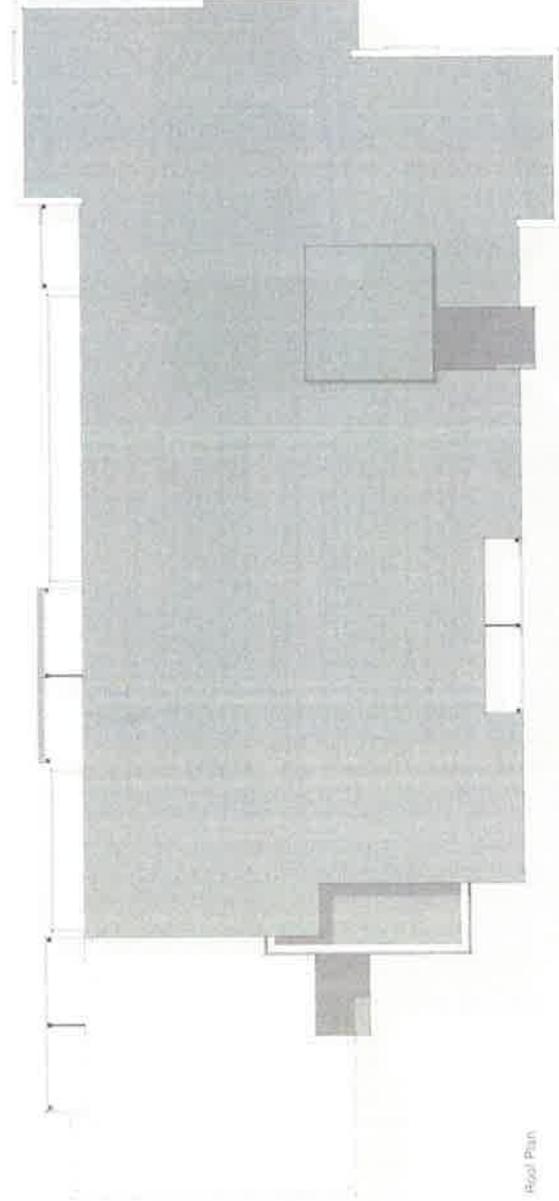
# Agenda Item 5



Front Courtyard Elevation



Right Flank Elevation



Roof Plan



South View 1

# Agenda Item 6

Planning Committee  
24<sup>th</sup> November 2021

Agenda Item: 6  
21/01367/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	24 <sup>th</sup> November 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Lesley Westphal
	<b>TELEPHONE:</b>	01737 276769
	<b>EMAIL:</b>	Lesley.westphal@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	6	<b>WARD:</b> NORK

<b>APPLICATION NUMBER:</b>	21/01367/F	<b>VALID:</b>	14 July 2021
<b>APPLICANT:</b>	Rushmon Ltd	<b>AGENT:</b>	RDJW Architects
<b>LOCATION:</b>	<b>136-140 BRIGHTON ROAD, BURGH HEATH, SURREY KT2 6AQ</b>		
<b>DESCRIPTION:</b>	<b>Demolition of 138 and erection of 1 bungalow, 1 semi detached house and 8 maisonettes with associated access, parking and amenity space.</b>		
<b>DRAWING NUMBERS:</b>	All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.		

## SUMMARY

This is a full application for the erection of 1 bungalow, 8 maisonettes and 1 semi detached house (total 10) on a residential site that has already been cleared of the previous bungalow. It lies in the urban area.

The site has a lengthy planning history with planning permission finally being granted on appeal pursuant to a 2020 application for 1 bungalow, 6 maisonettes and two terraced houses (total 9) with associated access, parking and amenity space.

The scheme seeks to make modest alterations to the approved scheme, replacing one terraced house with 2 maisonettes and increasing the parking provision by two spaces,. The design, layout, means of access, amenity space and general character of the scheme remains very similar to that already approved.

The Planning Inspector considered that the density, scale, mass and general character of the scheme would be acceptable such as to uphold the appeal. The proposed change introduces two additional maisonettes and would result in very little change to the elevations of the approved scheme - such that if noticeable from outside the site, they would not materially increase the bulk and mass of the block concerned such as to result in an unacceptable appearance nor such increased bulk and mass as to be unacceptable.

The approved scheme has a shortfall of 4 parking spaces compared to the DMP standards and the parking provision in this scheme would retain that shortfall, but make it no worse. Accordingly, no objection is raised in this respect, given the planning

# Agenda Item 6

Planning Committee  
24<sup>th</sup> November 2021

Agenda Item: 6  
21/01367/F

history of the site. The precise details of the visibility splays are still under discussion with the county highways department, but this matter can be dealt with by means of an appropriate condition: as indeed was the case in the appeal.

The scheme is not considered to cause any greater impact upon the neighbours amenities or living standards of future occupants nor to retained trees around the site.

A number of conditions are proposed reflecting the fact that many of the issues identified by the approved scheme have been addressed and the relevant conditions discharged.

Overall, and when considering the previous scheme and appeal decision, the scheme is considered to be policy compliant.

## **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.

## Consultations:

Highway Authority: Final comments awaited following submission of plans showing two additional parking spaces and proposed sightlines for the access.

Officer Note: The two additional parking spaces would bring the shortfall against published standard to 4 spaces.

Surrey Historic Environment: The proposal falls outside of the assessment requirements set out in the Local plan and it would also be difficult to justify archaeological condition under national (NPPF) policy

## Representations:

Letters were sent to neighbouring properties on 3 September 2021. Two responses have been received raising the following issues:

Issue	Number	Response
Inadequate parking		See paragraph 6.15-6.19
Over development		See paragraph 6.3-6.9
Poor design out of character with surrounding area		See paragraph 6.3-6.9
Poor standard of amenity for residents of the proposed bungalow		See paragraph 6.5 & 6.16
Garden sizes out of character with surrounding area		See paragraph 6.5
Harm to trees		See paragraphs 6.20- 6.24
Ham to neighbours amenities		See paragraph 6.10-6.14

## 1.0 Site and Character Appraisal

1.1 The application site comprises of the whole of 138 Brighton Road and parts of the rear gardens of 136 and 140 Brighton Road. The site has now been cleared. It is generally level with a mature tree screen at the rear and abutting existing residential development to the north and south.

1.2 This section of the Brighton Road is served by a slip road that runs parallel to the road. The area is characterised by modest sized, detached bungalows, set away from the roadway on deep plots. Some of the mature belt to the rear of the gardens are covered by tree preservation orders.

## 2.0 Added Value

2.1 Improvements secured at the pre-application stage: The applicant did not approach the Council for pre-application advice therefore the opportunity to secure improvements did not arise

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2.2 Improvements secured during the course of the application: Improvements to the parking layout and numbers and access arrangements

2.3 Further improvements could be secured: Proposed conditions will secure a range of details including tree protection, acceptable materials and a safe access.

## 3.0 Relevant Planning and Enforcement History

3.1	07/00522/F	Demolition of existing building (No 142) and erection of 5 No. new detached houses, 4 with integral garages and 1 with detached single garage. New access road.	Refused. Appeal Dismissed.
3.2	07/01441/F	Demolition of existing building (No 142) and erection of 5 No. new detached houses, 5 with integral garages. New access road	Refused. Dismissed on appeal
3.3	07/01943/F.	Demolition of existing house & erection of 4 residential units with integral garages	Refused. Dismissed on appeal
3.4	14/01595/OUT - . -	Demolition of 138 Brighton Road and erection of 4 dwellings on land to the rear of 134-140 Brighton Road	Withdrawn by applicant.
3.5	15/01170/OUT	Demolition of 138 and the erection of 3 detached dwellings with associated access, garaging and amenity space on the land at and to the rear of 134 - 140 Brighton Road	Refused
3.6	20/00314/F	Demolition of 138 and the erection of 1 detached bungalow, 2 terrace houses and 6 maisonettes with associated access, parking and amenity space	Refused. Allowed on appeal
3.6	21/00864/S73	Demolition of 138 and erection of 1 detached bungalow, 2 terrace houses and 6 maisonettes with associated access, parking and amenity space: amendment sot elevational treatment	Approved

## 4.0 Proposal and Design Approach

- 4.1 This is a full application for the demolition of 138 Brighton Road, which has already taken place and the erection of 13-bed detached bungalow, 1 semi detached 3 bed house and 8 x 2 bed maisonettes. The application is very similar to applications reference 20/00314/F and 21/00864/S73 and essentially seek to swap one of the proposed terrace units for two additional apartments, along with other minor changes
- 4.2 The design of the proposed scheme would be almost identical to the scheme already approved, siting a detached bungalow at the front of the site partially over the footprint of the original dwelling set close to the southern boundary of the site. It would be set back behind the front and rear line of the adjacent property 140 Brighton Road, as previously approved.
- 4.3 The access road would lie between the bungalow and the flank of 136 Brighton road providing access to a two storey block containing 4 maisonettes and one semi detached 3 bedroom house which would lie to the rear of 136 Brighton Road. To the south of that block would lie a two storey block containing 4 maisonettes, lying to the rear of 140 Brighton Road.
- 4.4 Parking would be located around the site – 2 spaces in front of the bungalow, two spaces adjacent to the semi detached unit , 11 spaces in front of the block of 4 maisonettes and semi detached unit and two in front of the southernmost maisonette block.
- 4.5 The design of these units is fairly traditional with brick faced elevations, casement windows and pitched tiled roofs and remains as previously approved.
- 4.6 Amenity space for the bungalow lies to the rear of that unit, for the maisonettes and semi detached unit to the rear and side of each block and the semi detached house has a modest front garden and amenity space to the rear.
- 4.7 This scheme varies from the previous approval insofar as:
- the block to the rear of 136 Brighton Road would accommodate 4 maisonettes and 1 semi detached unit instead of 2 maisonettes and two semi detached units. The block would be in the same place but the two storey element with the maisonettes would move just over 1 m closer to the rear of number 136.
  - Minor alterations to the window arrangement to accommodate this change
  - Two additional parking spaces have been created by reducing slightly the planting bed in front of the semi detached unit and removing a narrow planting strip within the main parking area and their replacement with one space and one additional space adjacent to the semi detached unit.

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- The elevational treatment of the bungalow would be slightly different to the front (narrower window span on the front elevation, new rooflight on the front elevation, solar panels on the rear roofspace and reduction in span of patio doors on the flank elevation)
- The boundary line to the rear of 136 Brighton Road has been straightened and slightly shortens the garden space in the north east corner compared to the approved plan.
- Amendments to plot sizes resulting in slight increases/decreases to plots 6-10.

4.8 Further details of the development are as follows:

Site area	0.29ha
Existing parking spaces	15 spaces in 2020 approval
Proposed parking spaces	17 spaces now proposed
Parking standard	Low accessibility
Net increase in dwellings	1 compared to previously approved scheme
Existing site density	31dph (approved scheme)
Proposed site density	34.5 dph
Density of the surrounding area	7.1 (ex.adjacent bungalows)

## 5.0 Policy Context

### 5.1 Designation

Urban area

Low accessibility area (4)

CIL Charging area 2 £140/sq.m.

Surface Water Flood Area on the Brighton Road (but not the site or the slip road) 1:30 years and 1:1000years

Tree Preservation order BAN192 Mixed Species at rear of site.

- 5.2 Reigate and Banstead Core Strategy 2004;  
CS1, Presumption in Favour of Sustainable Development  
CS10, Sustainable Development  
CS11 Sustainable Construction  
CS13, Housing Delivery  
CS14, Housing Needs of the Community  
CS17 Travel Options and Accessibility

- 5.3 Reigate and Banstead Development Management Plan 2019:  
DES1, Design of New Development  
DES2, Residential Garden Land Development  
DES4, Housing Mix  
DES5, Delivering High Quality Homes

DES6, Affordable Housing  
DES8, Construction Management  
TAP1, Access, Parking and Servicing  
CCF1, Climate Change Mitigation  
NHE2, Protecting and Enhancing Bio diversity and Areas of Geological Importance  
NHE3, Protecting Trees, Woodland Areas and Natural Habitats  
NHE9: Heritage Assets  
INF3. Electronic Communications Networks

#### 5.4 Other Material Considerations

Supplementary Planning Guidance      Surrey Design  
Local Distinctiveness Design Guide  
SPD 2021

Other      Human Rights Act 1998

### 6.0 **Assessment**

6.1 This application seeks full planning permission for the erection of 1 bungalow, 8 maisonettes and 1 semi detached house with associated parking, access and amenity space. The scheme is similar to previously approved development on this site and the development of the site for residential purposes is acceptable in principle.

6.2 The main issues to consider are:

- Design and impact upon the character of the area
- Neighbour amenity
- Access and parking
- Trees
- Surface Water Flooding
- Archaeology

#### Design and Impact upon the character of the Area

6.3 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.

6.4 Policy DES2 relates to Residential Garden Land development and is a criteria based policy setting out a number of criteria against which development will be

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assessed including consideration of the scale, form and external materials in the locality, height, bulk and mass of existing buildings, plot sizes and widths, significant landscape features including mature trees, the provision of a good standard of amenity and the general character of the area.

- 6.5 The redevelopment of the site in a manner very similar to that now proposed has been considered at appeal when the Inspector concluded that :
- The bungalow although close to the southern site boundary would not appear unacceptably cramped nor result in undue disruption to the character and appearance of the existing street frontage
  - The general height and massing of the two storey dwellings would not be so substantial to appear as being visually dominant in their surroundings, nor prominent or intrusive within the streetscene.
  - The parking area whilst resulting in some limited internal harm in terms of the overall design quality, would not be visible or detrimental in the wider public domain
  - Spacing between and around the buildings or between existing and proposed buildings would not be unacceptably cramped or result in incongruous development
  - Although the plot sizes would be smaller than those around the site, any actual harm arising from this would be minimal given their location to the rear of the frontage dwellings and away from public vantage points.
  - The scheme would result in an acceptable living environment for future residents of the bungalow: with no unacceptable overlooking or unacceptable impacts from noise and disturbance from the access road: vehicles being unlikely to pass at great speed and with limited frequency.
- 6.6 The scheme now proposed varies very little compared to the scheme considered by the Inspector in the above terms. The changes are set out above (para 4.7) and would result in little discernible difference in appearance and bulk, scale and mass compared to the originally approved scheme. The most noticeable difference would be the slight change to the bulk of the block to the rear of 136 Brighton Road, but that would be relatively minor and would not materially affect the views of the site from within or outside the site.
- 6.7 The main parking area would be altered to accommodate one additional space whilst one additional space would be added alongside the semi detached unit. Neither of these changes would result in a significantly altered appearance or character to the scheme.
- 6.8 The proposed changes to plot sizes 6-10 as a result of the changes proposed to the main block would be minimal and would not affect the character of the scheme from within or from outside the site.
- 6.9 Overall it is considered that the scheme would not be significantly different to that approved on appeal and is considered to comply with the Development Plan.

## Neighbour Amenity

- 6.10 DMP Policy DES1 requires new development to provide an appropriate environment for future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings, including by way of overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy. This guidance is reiterated in Policy DES2.
- 6.11 The relationship would vary little between that approved on appeal and that now proposed. The most significant changes noticeable to residents outside the site would be the change to the boundary line to the rear of 136 Brighton Road, which has been straightened and reduces very slightly the amenity space available to the rear of number 136 and the proposed positioning of solar panels on the roofspace facing 140 Brighton Road.
- 6.12 The fact that solar panels would be visible on the proposed bungalow to the adjacent residents is not considered materially harmful: not adding significant bulk or mass to the roofline such as to be harmful. The straightening of the boundary line at the rear of 136 Brighton Road would result in a marginally smaller garden to that property, but would not be so much smaller as to be inadequate or unacceptable. The change to this boundary would allow an additional parking space to be provided to the semi detached house which lies at the rear of 136 so there would be two parking spaces immediately adjacent to the fence line. However the separation distance between these spaces and the rear of 136 (14m's) would be sufficient to ensure that a significant adverse impact from noise would not occur to the existing residents.
- 6.13 The additional parking spaces required and therefore additional traffic drawn to the site would be minimal and would not materially affect the amenities of the surrounding residents, either in terms of noise and disturbance as cars use this site nor in terms of additional traffic on the slip road and surrounding highway.
- 6.14 Overall it is considered that this scheme would be compliant with the Development Plan and would not cause harm to the amenities of either existing residents nor those future residents of this scheme.

## Access and Parking

- 6.15 Policy TAP1 requires new development to provide safe and convenient access for all road users taking account of cumulative impacts. The previous scheme required 20 spaces and provided 16, this application requires 22 and provides 18 and so it 'washes its own face' in terms of catering for the additional parking demand.
- 6.16 The County highways Authority have previously assessed the scheme on this site and did not raise objection to the layout, access nor the shortfall of 4 spaces, when assessed against the identified parking standards for this area. A reason for refusal on highways grounds did not form part of the delegated refusal on application reference 20/00314/F. Although residents had raised

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this issue as a matter of concern, the Inspector only commented upon parking and highways issues as below:

- to conclude that the proximity of the access road through the site would be unlikely to generate such traffic as to result in unacceptable impacts from noise and disturbance for future occupiers of the proposed bungalow.
- To conclude that the large and dominant hardstanding immediately in front of a building would detract to a limited extent from the overall quality of the overall residential environment
- To conclude that reasonable visibility would be achieved and that a condition could ensure that an appropriate access could be achieved. The traffic generated by 9 dwellings would not be substantial and he was satisfied that the proposal would not result in any unacceptable highways impacts.

6.17 This scheme has been amended to provide two additional spaces such as to result in a shortfall of 4 parking spaces compared to the minimum required by the parking standards. This equates to the same shortfall as previously and so there is no additional harm caused. Case law (Chisnell vs LB of Richmond) has been quoted in respect of the extent to which the former applications represent a material consideration. However, in that case there was no previous approval unlike here. Given the extant permission comprises a legitimate fallback position for this site (there being approximately 2 ½ years left for implementation), it is not considered reasonable to find the current proposal unacceptable due to perceived deficiencies of the former and when there is no additional harm caused.

6.18 At the time of writing this report the access and visibility was under discussion with the County Highways Department. It is noted that condition 8 of the Appeal Decision required that prior to occupation of any part of that development that a vehicular access to the highways should be constructed in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority. Such details were also to include the closure of the existing access to and from the site. In the absence of comments finalising highways views on this site, it is considered that the means of access can be addressed in the same way, ie by condition.

6.19 In conclusion although the scheme has changed and includes one additional dwelling and two additional spaces compared to the previously approved scheme, it is not considered that this scheme would result in a materially different impact upon the highway such as to justify a refusal. Subject to conditions regarding the provision of parking and details of the access the scheme is considered to be acceptable.

## Impact upon Trees

6.20 Trees, hedges and woodland areas make a particularly valuable contribution to the character and visual amenity of the borough, both in the townscape and the landscape. They can also be valuable for biodiversity, providing important habitats for local wildlife and as part of wildlife corridors; and are

important for flood resilience. DMP Policy NHE3 seeks to safeguard valuable tree cover, and ensure that, through good design and best practice, these features are protected and enhanced as an integral part of new development.

- 6.21 The Council has previously refused a scheme similar to this on the grounds of concerns that it would result in the development lying in such close proximity to the development as to lead to unreasonable pressure to reduce the height of, or remove trees that make a positive contribution to the character of the area.
- 6.22 The Inspector acknowledged that trees of reasonable and good amenity value are located towards the rear of the site and that part of the scheme would lie in close proximity to existing trees. However the submitted evidence concluded that this would not adversely affect retention of the trees nor that dwellings would be in such close proximity as to unacceptably affect levels of daylight/sunlight and outlook to individual dwellings. The Inspector concluded that the scheme would not create unacceptable pressures for the removal or significant pruning of existing trees of significance .
- 6.23 The proposed scheme would not make a significant difference to the relationship of the development to the trees: the only material difference being that one additional parking space to the semi detached unit would lie partially within the root protection area of two nearby retained. However this would not be a significant intrusion and would not cause long terms risks to the health of the trees.
- 6.24 Accordingly it is not considered that the changes proposed would result in an unacceptable impact upon future residents or trees nor lead to pressures to significantly prune or remove existing trees. The scheme is therefore considered to comply with the Development Plan.

### Surface Water Flooding

- 6.25 Policy CCF1 requires that new development ensure that the development itself is protected from flood risks and that it does not increase flood risks elsewhere.
- 6.26 Concerns have been expressed about the potential for water to drain off the site from the access road that may contribute to surface water flood issues on the nearby highway. The Inspector concluded that any concerns regarding this issue could be addressed by condition so that details of surface water drainage be submitted to the LPA prior to occupation, with the potential for a sustainable drainage system being investigated as part of that submission.
- 6.27 On that basis the scheme is considered to be policy compliant

### Archaeology

- 6.28 Policy NHE9 requires development to protect, preserve and wherever possible to enhance designated and non-designated assets. An

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archaeological assessment including where appropriate a field evaluation will be required to inform the determination of applications for sites which affect or have the potential to affect Scheduled Ancient Monuments, areas of Archaeological Importance or High Archaeological Potential and all other development sites exceeding 0.4ha.

- 6.29 This site does not comprise a site within or affecting any of the designations referred to in NHE9. Evidence is available of artefacts (Palaeolithic) having been recovered in the vicinity of the site which the County Council are aware of and they have suggested that the area is clearly of potential which is something that will be considered when the next County AHAP review is carried out. The difficulties associated with attaching a condition requiring a scheme of archaeological work are noted by the County Archaeologist as being very difficult to defend under the current policies, especially in the light of recent changes to the planning process requiring developers' agreement to conditions beforehand. Consequently, the most recent advice is that since the site does not fall within any of the relevant categories identified by the DMP that no archaeological concerns are expressed.
- 6.30 On the basis of this advice the application is considered to comply with the Development Plan without the justification to attach an archaeological condition.

## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Reason:  
To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.
2. The development hereby permitted shall be carried out in accordance with the following approved plans.

Reason: To define the permission and ensure the development is carried out in accordance with the approved plans and in accordance with National Planning Practice Guidance.

Plan Type Received	Reference	Version	Date
Location Plan	5547-001	A	19 May 2021
Block Plan	5547-002	A	19 May 2019
Ex Site Layout	5547-003	A	19 May 2021
Ex combined plan	5547-004(1)		19 May 2021
Prop Combined Plan Plot	11296T-T005		3 Sept 2021
Prop Combined Plan	5547-007	G	19 May 2021
Prop Combined Plan	5547-008	J	15 July 2021

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Prop Combined Plan	5547-009		19 May 2021
Prop Vision Splay			21 October 2021
Prop Section	5547-010	B	19 May 2021
Prop Site Layout	5547-005S		21 October 2021
Prop Streetscene	5547-011	A	19 May 2021

3. No development above ground level shall take place until samples of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with such approved materials. Alternatively, the scheme shall be implemented in accordance with the details agreed pursuant to application reference 21/00864/DET03.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

4. No development shall take place until full details of the finished levels, above ordnance datum, of the ground floors of the proposed buildings, in relation to existing ground levels have been submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved levels. Alternatively, the scheme shall be implemented in accordance with the details agreed pursuant to application reference 21/00864/DET04.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

5. No part of the development shall be occupied until boundary fences, walls or screens have been erected in accordance with details (including positions, design and materials) which shall have previously been submitted to and approved in writing by the local planning authority. The approved boundary treatment shall thereafter be retained in accordance with such approved details.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the policy DES1 of the Reigate and Banstead Development Management Plan 2019.

6. The development shall be carried out in accordance with the arboricultural protection measures contained within the Arboricultural Report prepared by DPA Arboricultural Consultants dated May 2021. No development shall commence, including groundworks preparation and demolition, until the relevant tree protection and monitoring measures are in place.

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Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction - Recommendations' and policies DES1 and NHE3 of the Reigate and Banstead Development Management Plan 2019.

7. (i) No development shall commence until a scheme for the landscaping of the site, including the retention of existing landscape features, has been submitted to and approved in writing by the local planning authority. The scheme shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities along with a implementation and management programme.

(ii) All hard and soft landscaping work shall be completed in accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development or in accordance with a programme which shall have previously been submitted to and approved in writing by the local planning authority. Maintenance shall be subsequently carried out in accordance with the approved management programme.

(iii) Any trees, shrubs or plants which are removed, die or become seriously damaged or diseased within a period of five years from implementation.

Alternatively the scheme shall be implemented in accordance with details approved pursuant to application reference 21/00864/DET07

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with Reigate and Banstead Development Management Plan 2019 policies DES1 and NHE3.

8. No part of the development shall be occupied until a vehicular access to the highway has been constructed in accordance with details which shall have been previously submitted to and approved in writing by the local planning authority. Such details shall also include the closure of the existing vehicular access to and from the site.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policies DES1 and TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

9. No dwelling shall be occupied until space has been laid out within the site in accordance with drawing no. 5547-005S for cars to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained at all times for those purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policies DES1 and TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

10. The development hereby approved shall not be occupied unless and until each of the dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in recognition of Section 9 "Promoting Sustainable Transport" in the National Planning Policy Framework 2019 to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

11. No development shall commence until a Construction Transport Management Plan (CTMP), to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) provision of sightlines behind any boundary fencing
  - (e) measures to prevent the deposit of materials on the highway

The approved CTMP shall be adhered to throughout the demolition and construction period for the development

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policies DES1 and TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

12. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
  - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

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Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

13. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
  - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

14. No dwelling hereby permitted shall be occupied until surface water drainage works have been implemented in accordance with details that shall first have been submitted to and approved in writing by the local planning authority. Before any details are submitted to the local planning authority an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system, having regard to Defra's non-statutory technical standards for sustainable drainage systems (or any subsequent version), and the results of the assessment shall have been provided to the local planning authority. Where a sustainable drainage scheme is to be provided, the submitted details shall include the following:

i) provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;

ii) include a timetable for its implementation; and,

iii) provide, a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The sustainable drainage system shall be managed and maintained thereafter in accordance with the agreed management and maintenance plan

Reason: To ensure the design meets the technical standards for SuDs and the final drainage design does not increase flood risk on or off site in accordance with policy CS10 of the Core Strategy 2014, policies DES9 and CCF2 of the Development Management Plan 2019 and the 2019 NPPF.

15. Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of the management company and state the national grid reference of any key drainage elements (surface water attenuation devices / areas, flow restriction devices and outfalls).

Reason: To ensure that the Sustainable Drainage System has been constructed as agreed to the National Non-Statutory Technical Standards for SUDS and to prevent flooding with regards to policy CS10 of the Core Strategy 2014 and policy CCF2 of the Development Management Plan 2019.

16. The development shall be implemented in accordance with the recommendations, avoidance and mitigation measures identified in the AAE Environmental Consultant's letter dated 30 January 2020 (ref: 193435/JDT). All ecological enhancement measures shall be completed prior to first occupation of the development, with confirmation of such completion provided in writing to the local planning authority.

Reason: To ensure that the development would not harm wildlife or protected species and deliver a biodiversity enhancement in accordance with Policy NHE2 of the Development Management Plan, Natural England standing advice and the provisions of the NPPF.

17. The development shall not be first occupied until a waste management collection point has been provided in accordance with details which shall have been submitted to and approved in writing by the local planning authority. The waste management collection point shall thereafter be maintained for this use.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage in accordance with Policy DES5 of the Reigate and Banstead Development Management Plan 2019.

18. Plots 3 and 5 of the development hereby approved shall not be occupied until obscure glazed fixed shut windows are installed as indicated on drawing no. 5547-007 G and no part of the windows shown to be obscure glazed that are less than 1.7m above the floor of the room in which each is installed shall be capable of being opened. These windows shall thereafter be retained as such.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the policy DES1 of the Reigate and Banstead Development Management Plan 2019.

19. Prior to the occupation of any dwelling, cycle parking shall be provided on site in accordance with details which shall have been previously submitted to and approved in writing by the local planning authority. The cycle parking shall thereafter be retained in accordance with the approved details

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Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and

- (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

5. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.

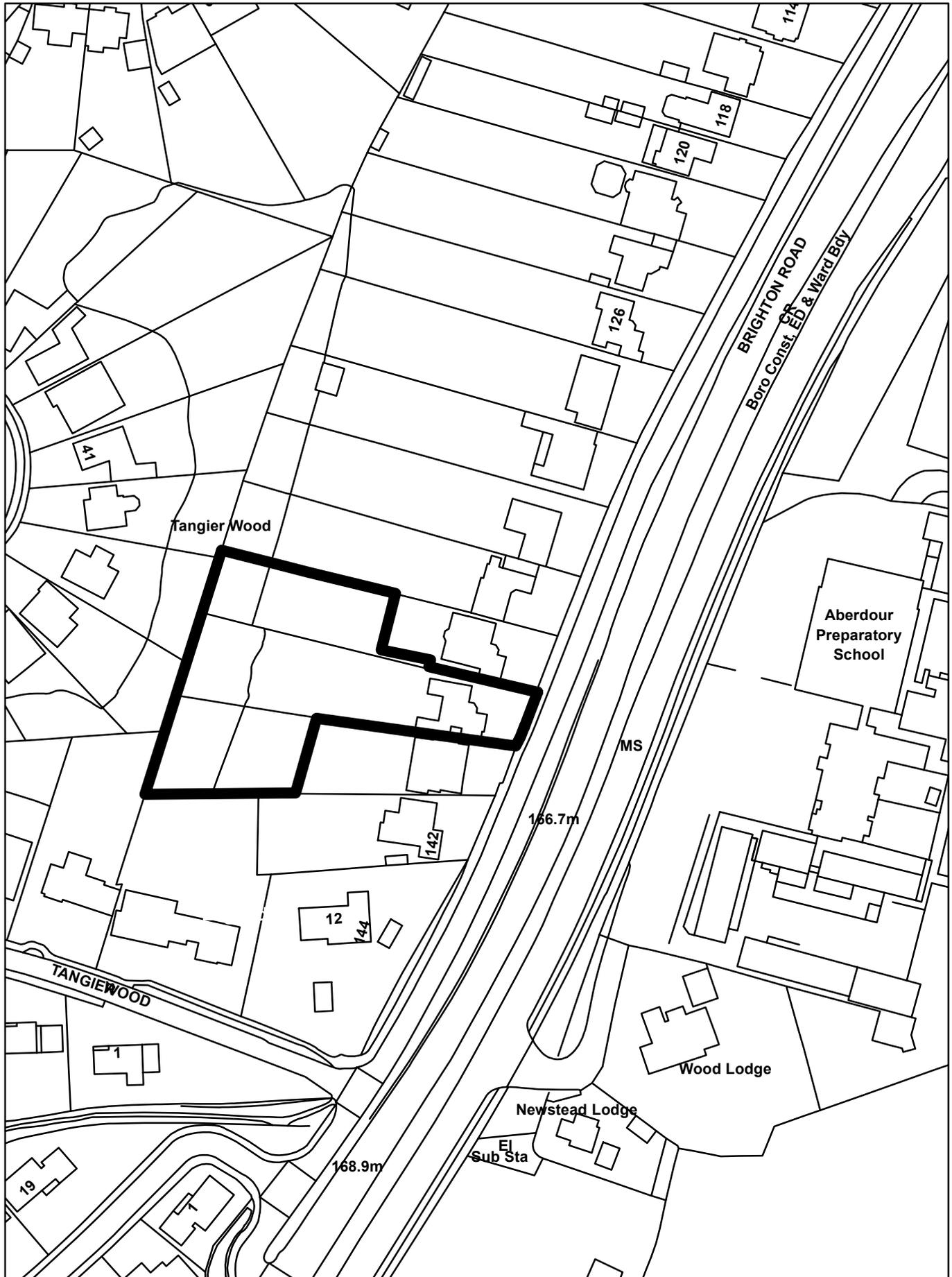
## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS10, CS11, CS13, CS14, CS17, DES1, DES2, DES4, DES5, DES6, DES8, TAP1, CCF1, NHE2, NHE3, INF3 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

Agenda Item 6  
21/01367/F - 136-140 Brighton Road, Burgh Heath



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**Drawing Revision History:**

A	Boundary updated to client comments.	06.05.21	MC
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Proposed Location Plan Scale 1:1250  
 0 25 50 75 100 125m  
 Scale 1:1250



Proposed Block Plan Scale 1:500  
 0 10 20 30 40 50m  
 Scale 1:500



**PLANNING DOCUMENT**

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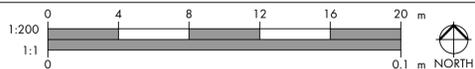
**RIBA**   
 Chartered Practice

**Project Title**  
 Proposed Residential Development  
 Brighton Road,  
 Tadworth

**Drawing Title**  
 Proposed Block & Location Plan

<b>Originator</b> AP	<b>Approved</b> 	<b>Date</b> 10.01.2020	<b>Scale</b> VARIES @ A1
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<b>Drawing No.</b> <b>5547-002</b>	<b>Rev.</b> <b>A</b>	<b>Sheet No.</b> <b>1/1</b>
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**Drawing Revision History:**

M	Bin lorry tracking shown	06.02.2020	AP
N	Schedule of accommodation amended	10.02.2020	AP
O	Amended to client comments	18.04.2021	MC
P	Further client amendments	20.04.2021	MC
Q	Client amendments to hard surfaces	09.06.2021	MC
R	Extra parking space added	05.10.2021	JG
S	Path extended	11.10.2021	MC

**Schedule of Accommodation**

8 No. Maisonettes 2 Bed units
1 No. detached chalet bungalow 3 bed
1 No. semi-detached 3 bed chalet house
<b>Total of 10 units and 22 beds</b>

**Parking Schedule**

2 spaces per 3 bed units = 4 spaces
1 space per 2 bed units = 8 spaces
4 visitors spaces
<b>Total of 16 spaces</b>

**Key**

- Managed amenity space
- Private garden
- Root protection zone
- Existing tree
- New tree
- New fence
- Concrete floor to Bin & Bike Store
- Block paving
- Tarmac Road
- Tarmac Path

bcp = Bin Collection Point  
 --- Previous Planning

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**PLANNING DOCUMENT**



**RIBA** Chartered Practice

**Project Title**  
 Proposed Residential Development  
 Brighton Road,  
 Tadworth

**Drawing Title**  
 Proposed Site Plan

<b>Originator</b> AP	<b>Approved</b> <i>MC</i>	<b>Date</b> 26.08.2019	<b>Scale</b> 1:200 @ A1
<b>Drawing No.</b> <b>5547-005</b>	<b>Rev.</b> <b>S</b>	<b>Sheet No.</b> <b>1/1</b>	



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**Drawing Revision History:**

A	Update to reflect latest scheme.	11.05.21	MC
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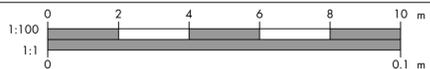


**RIBA**   
 Chartered Practice

**Project Title**  
 Proposed Residential Development  
 Brighton Road,  
 Tadworth

**Drawing Title**  
 Site Perspective Showing Street Scene

<b>Originator</b> AP	<b>Approved</b> 	<b>Date</b> 22.01.2020	<b>Scale</b> 1:100 @ A1
<b>Drawing No.</b> <b>5547-011</b>	<b>Rev.</b> <b>A</b>	<b>Sheet No.</b> <b>1/1</b>	



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**Drawing Revision History:**

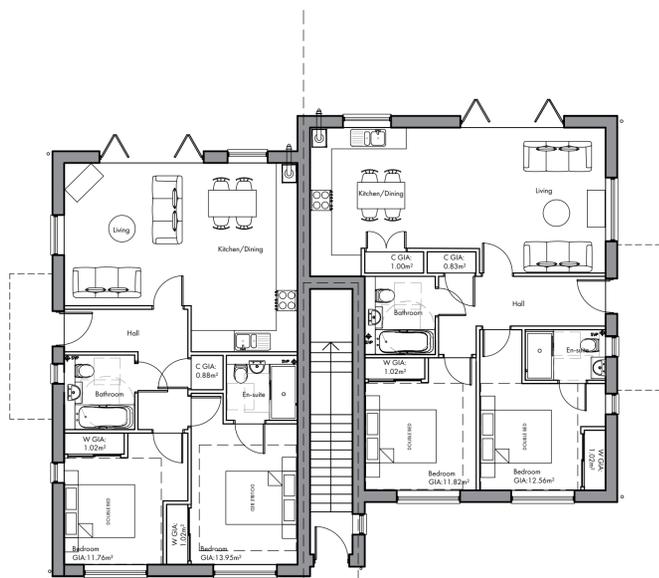
Rev.	Description	Date	By
A	Drawing updated	06.09.2019	AP
B	Drawing updated	08.10.2019	AP
C	Drawing updated, DWG no. changed	08.01.2020	AP
D	Flat roof height adjusted.	27.01.2020	AP
E	Openings and Kitchen amended to client comments.	29.03.2021	MC
F	Elevation amended to client comments.	31.03.2021	MC
G	Elevation amended to client comments.	10.05.2021	MC

**Key**

	Clay tiles colour: Grey
	Brickwork colour: Red
	Tile Hanging / Composite timber cladding
	Obscured glazing
	Brick plinth detail
	Previous Planning

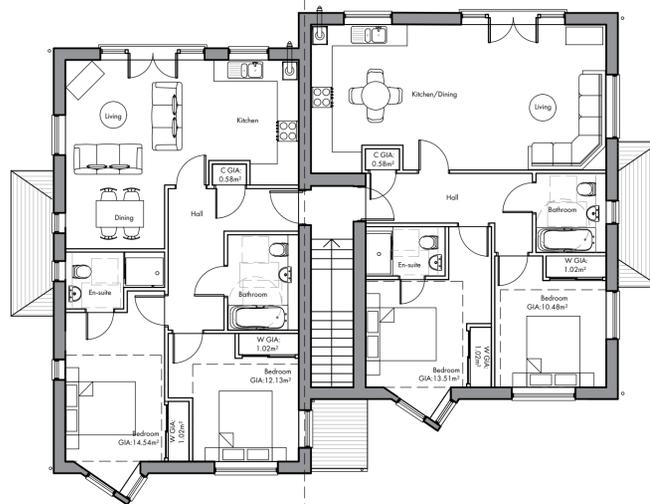


Hatching denoted first floor windows to be obscured and fixed shut



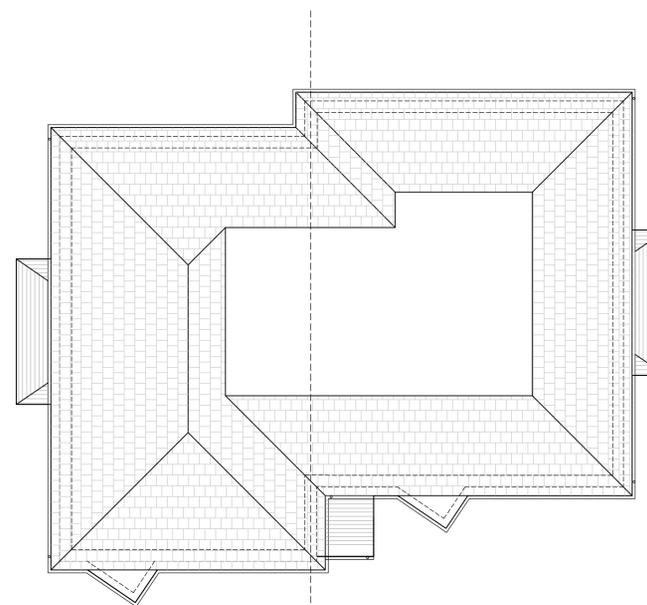
**PLOT 2**  
Ground Floor Plan  
GIA: 75.08m<sup>2</sup>

**PLOT 4**  
Ground Floor Plan Plot 4  
GIA: 76.44m<sup>2</sup>



**PLOT 3**  
First Floor Plan  
GIA: 75.91m<sup>2</sup>

**PLOT 5**  
First Floor Plan  
GIA: 77.29m<sup>2</sup>



**PLOT 2/3**  
Roof Plan

**PLOT 4/5**

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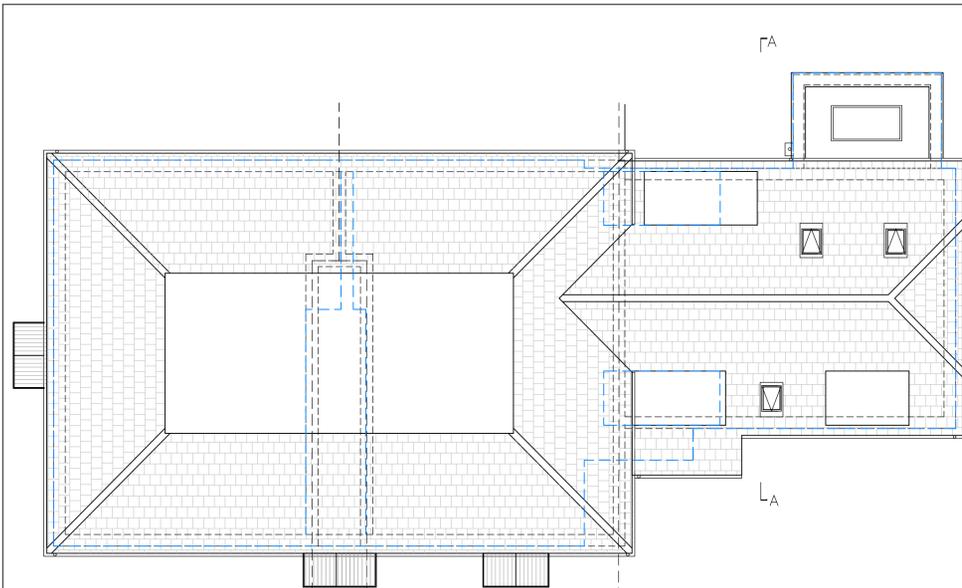


**RIBA**   
 Chartered Practice

**Project Title**  
 Proposed Residential Development  
 Brighton Road,  
 Tadworth

**Drawing Title**  
 Plots 2, 3, 4 & 5 - 2 Bedroom Maisonette  
 Proposed Floor Plans & Elevations

Originator	Approved	Date	Scale
AP		05.09.2019	1:100 @ A1
<b>Drawing No.</b>	<b>Rev.</b>	<b>Sheet No.</b>	
<b>5547-007</b>	<b>G</b>	<b>1/1</b>	



Roof Plan

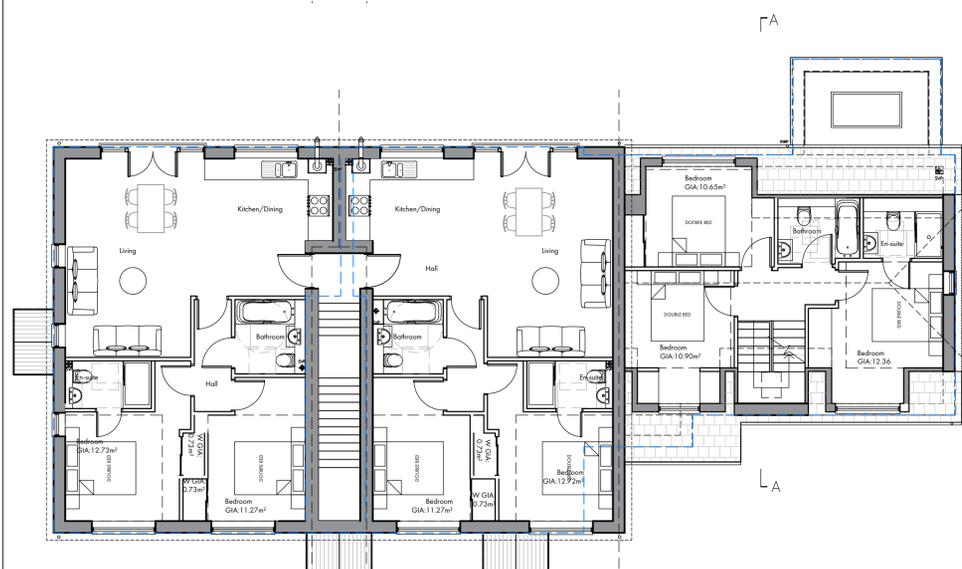


PLOT 6/7  
Front (South) Elevation

PLOT 8/9

PLOT 10

Section A-A



PLOT 7  
First Floor Plan  
GIA: 71.68

PLOT 9  
First Floor Plan  
GIA: 71.68

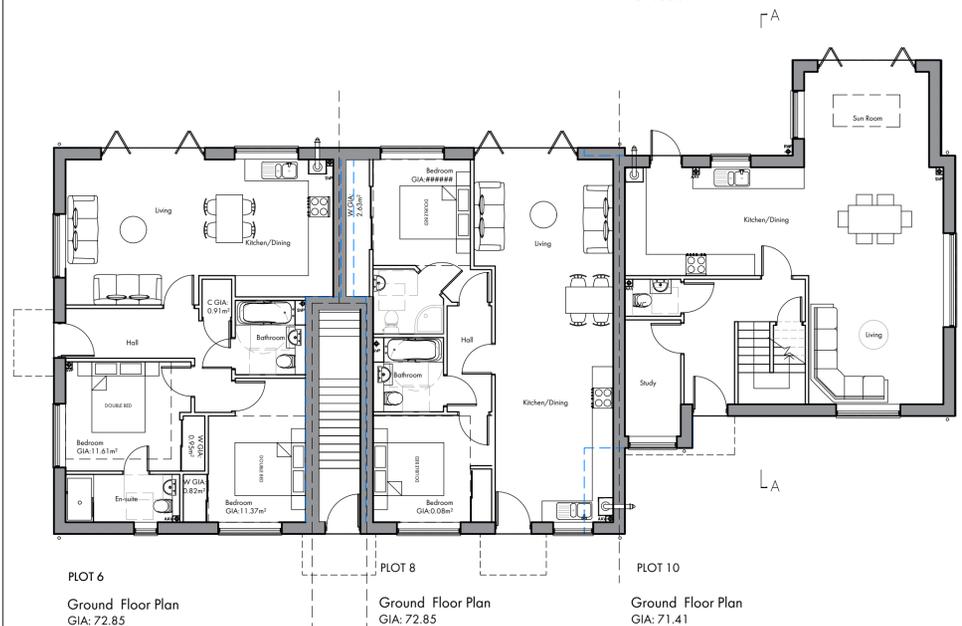
PLOT 10  
First Floor Plan  
GIA: 52.04



PLOT 10  
Rear (North) Elevation

PLOT 8/9

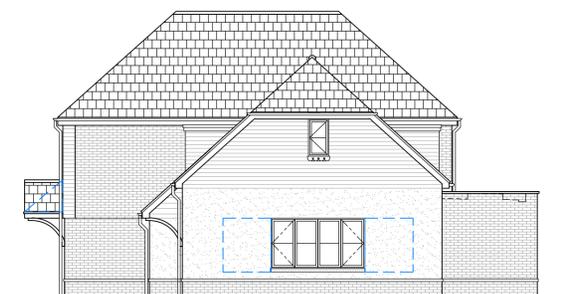
PLOT 6/7



PLOT 6  
Ground Floor Plan  
GIA: 72.85

PLOT 8  
Ground Floor Plan  
GIA: 72.85

PLOT 10  
Ground Floor Plan  
GIA: 71.41



PLOT 10  
Side (East) Elevation



PLOT 6/7  
Side (West) Elevation

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**Drawing Revision History:**

Revision	Description	Date	By
A	Drawing updated	06.09.2019	AP
B	Drawing updated	08.10.2019	AP
C	Drawing updated, DWG no. changed	08.01.2020	AP
D	Flat roof height adjusted.	27.01.2020	AP
E	Openings and Kitchen amended to client comments.	29.03.2021	MC
F	Adjusted to client comments.	30.03.2021	MC
G	Adjusted to client comments.	15.04.2021	MC
H	Adjusted to client comments.	20.04.2021	MC
I	Adjusted to client comments.	10.05.2021	MC
J	First floor area corrected.	12.07.2021	MC

**Key**

	Clay tiles colour: Grey
	Brickwork colour: Red/brown
	Tile Hanging / Composite timber cladding
	Render
	Brick plinth detail

--- Previous Planning

**PLANNING DOCUMENT**

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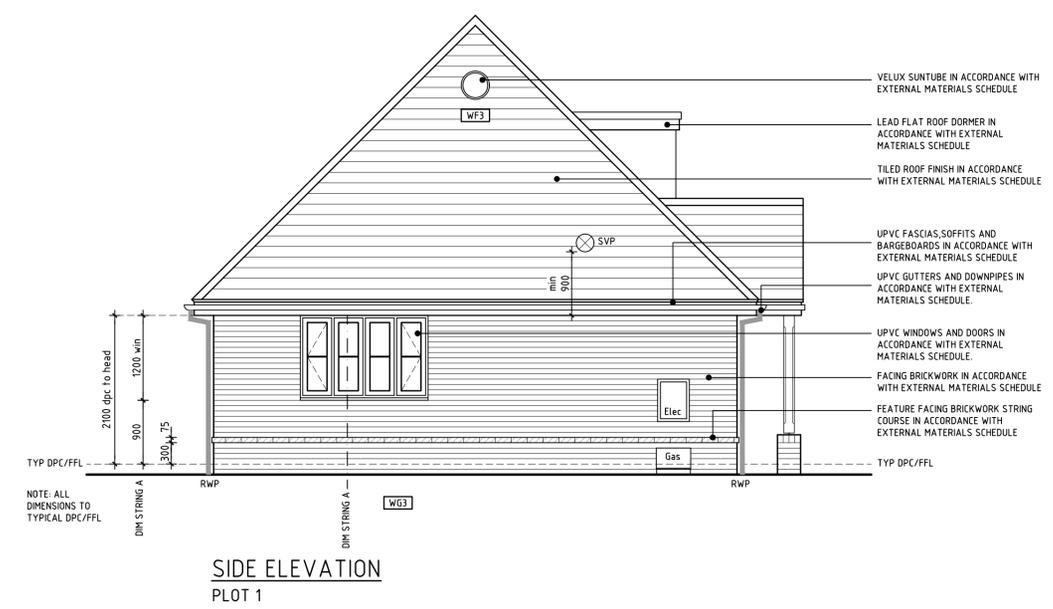
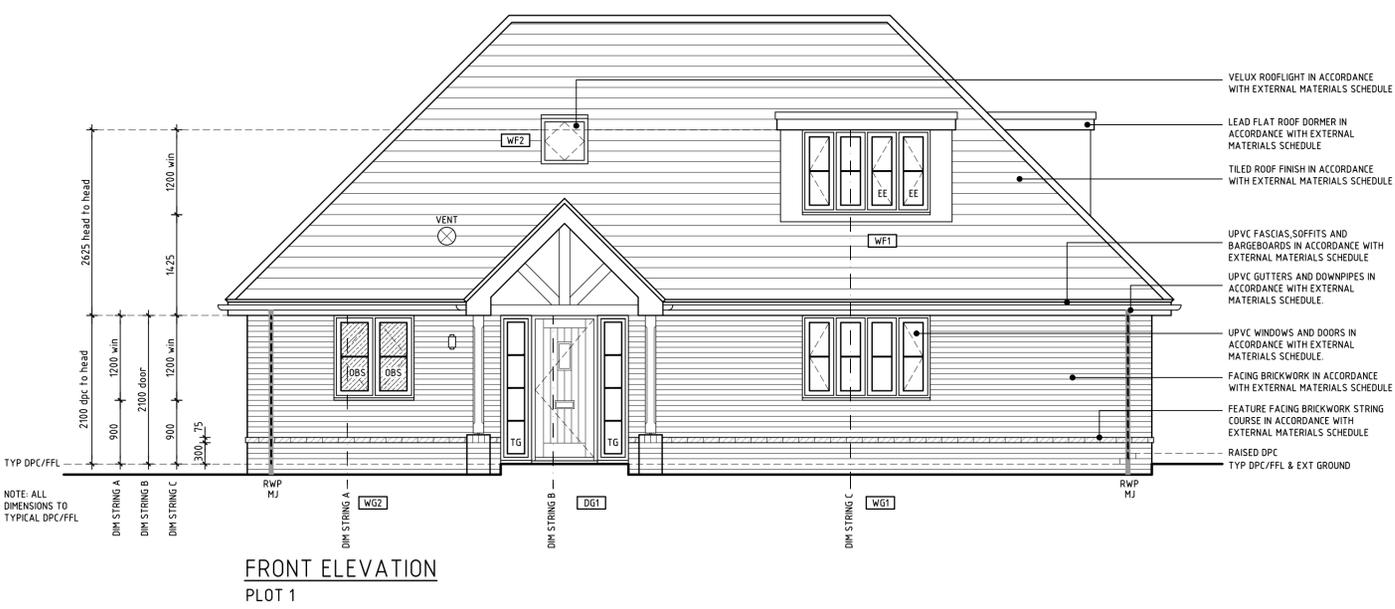
**RIBA** Chartered Practice

**Project Title**  
 Proposed Residential Development  
 Brighton Road,  
 Tadworth

**Drawing Title**  
 Plots 6 -9 2 Bed Maisonettes  
 Plot 10 - 3 Bed Chalet House  
 Proposed Floor Plans & Elevations

Originator	Approved	Date	Scale
AP		26.07.2019	1:100 @ A1

Drawing No.	Rev.	Sheet No.
<b>5547-008</b>	<b>J</b>	<b>1/1</b>



**ELEVATION LEGEND**

- BFT Boiler flue terminal (Note: all boiler flues to be located at a minimum of 300mm from any openable window)
- SVP SVP ducted to a thru tile vent at head of drain run only. Otherwise to terminate to durgo valve in roofspace. Refer to civil engineers design and details. SVP to terminate at least 900mm above the highest point of any opening into the building located within 3m
- Elec Wall mounted Elec meter box  
Cavity tray/ Lintel over and DPM behind. (Hockey sticks to be within cavity, not on external wall)
- Gas 'Uni-box' gas meter box
- Indicative wall mounted up/down light. Set out 1800mm from top of light fitting to external ground/door cill level

- ELEVATION NOTES**
- For location of Airbricks refer to Substructure layout
  - For details of roof pitches refer to sections and roofscape layout.

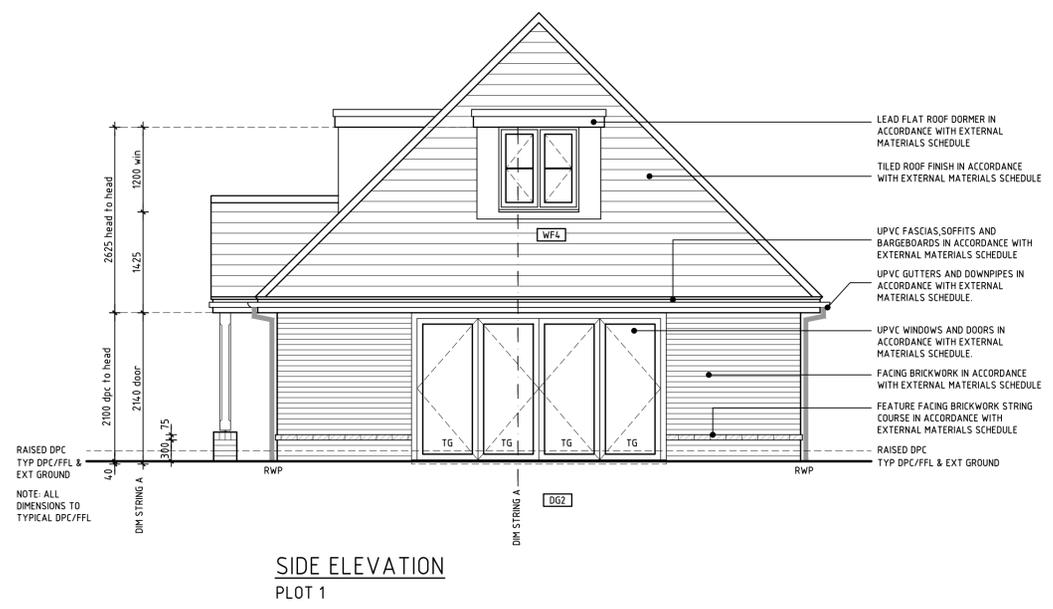
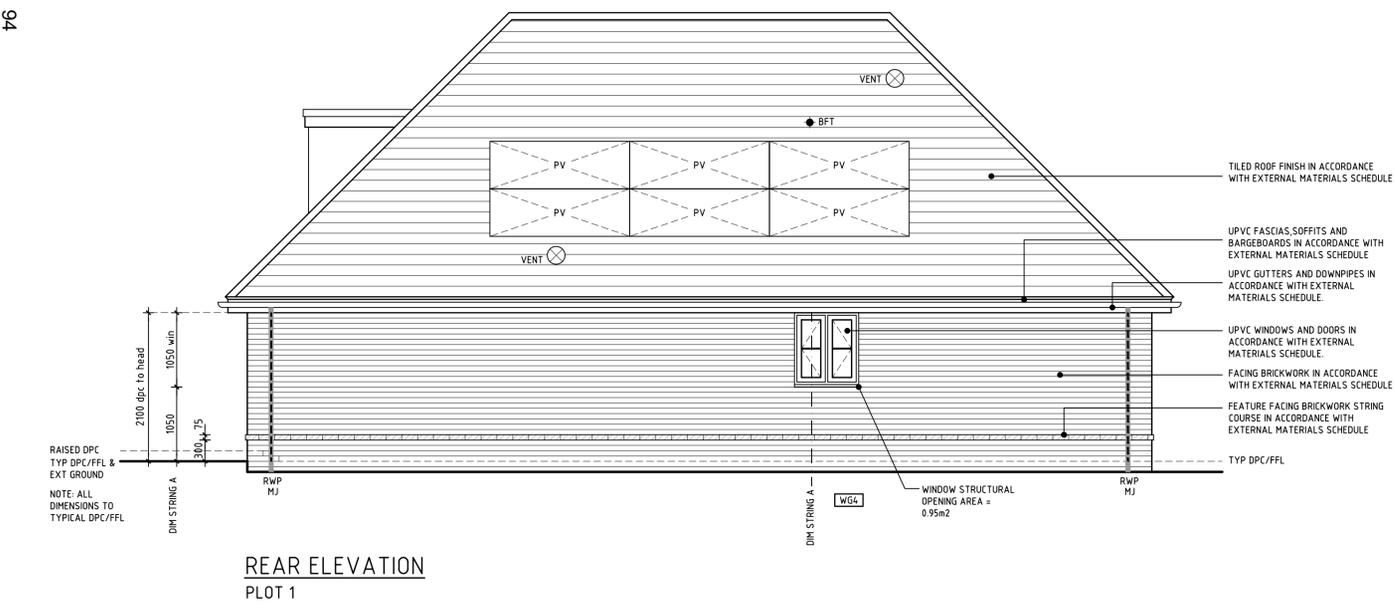
**ELEVATION ABBREVIATIONS**

- OBS Obscure glass  
To inner pane only.
- TG Toughened glass  
In accordance with A.D Part K4 'Protection against impact with glazing' 5.2a and BS EN 12600 and BS6206.
- EE Emergency Egress Window  
In accordance with A.D Part B12.8
- RWP Rainwater Pipe
- MJ Movement Joint
- SLF Stepped lead flashing linked to stepped cavity tray in wall.
- LF Lead flashing linked to cavity tray in wall.
- DPC Damp proof course  
Set min 150mm above finished ground level.

**VENTILATION LEGEND - SYSTEM 3**

Ventilation to be provided to comply with BR part F System 3 - continuously running independent extract fans. All in accordance with specialists design and details.

- VENT Extract taken to thru tile ventilator
- VENT Extractor ducted through to proprietary external airbrick or vent tile when located in tile hanging



Rev	Date	Details	Drawn
-	-	-	-

Client: **RUSHMON HOMES**

Site Address: BRIGHTON ROAD, TADWORTH

Description: PLOT 1 ELEVATIONS

Status: TENDER

Date: JULY 21 Scale: 1:50@A1

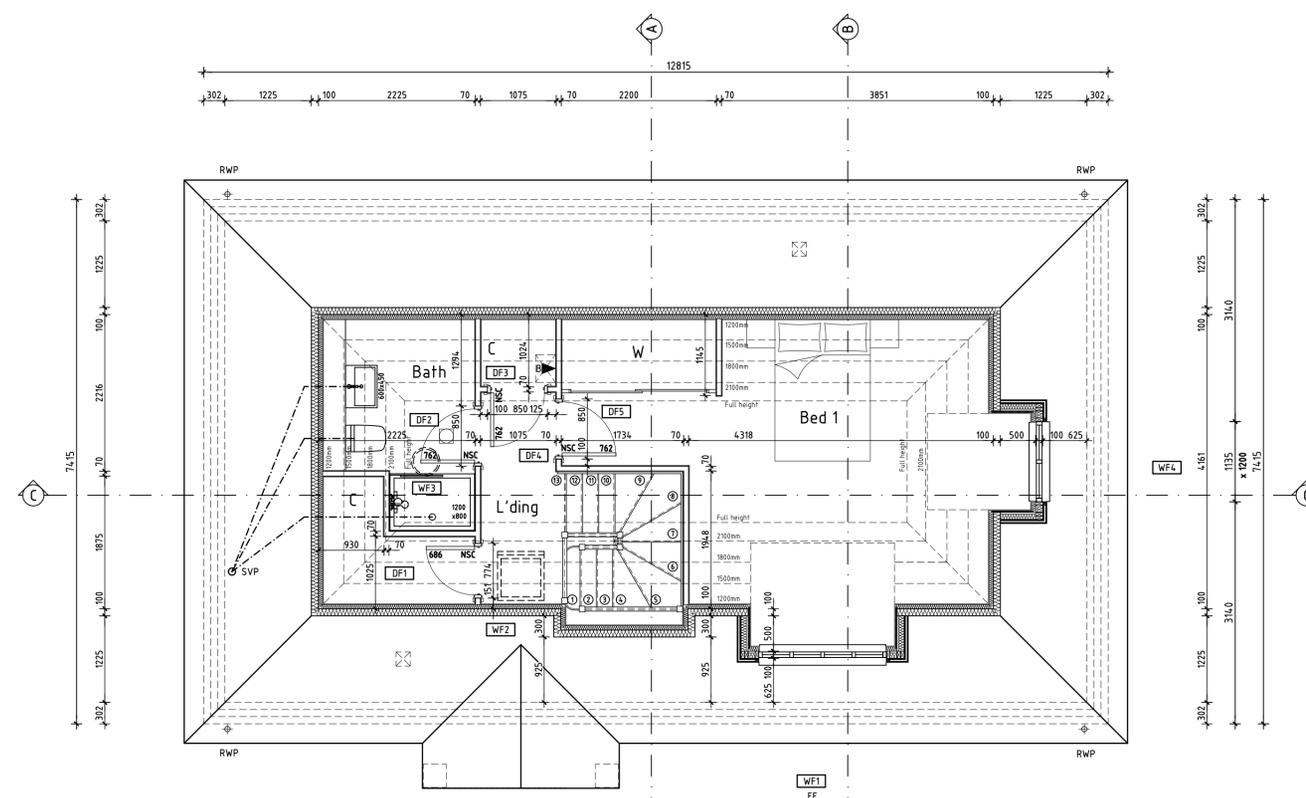
Drawn: MN Checked: TCA

Job Number: 1296 Drawing Number: T/005 Revision: -

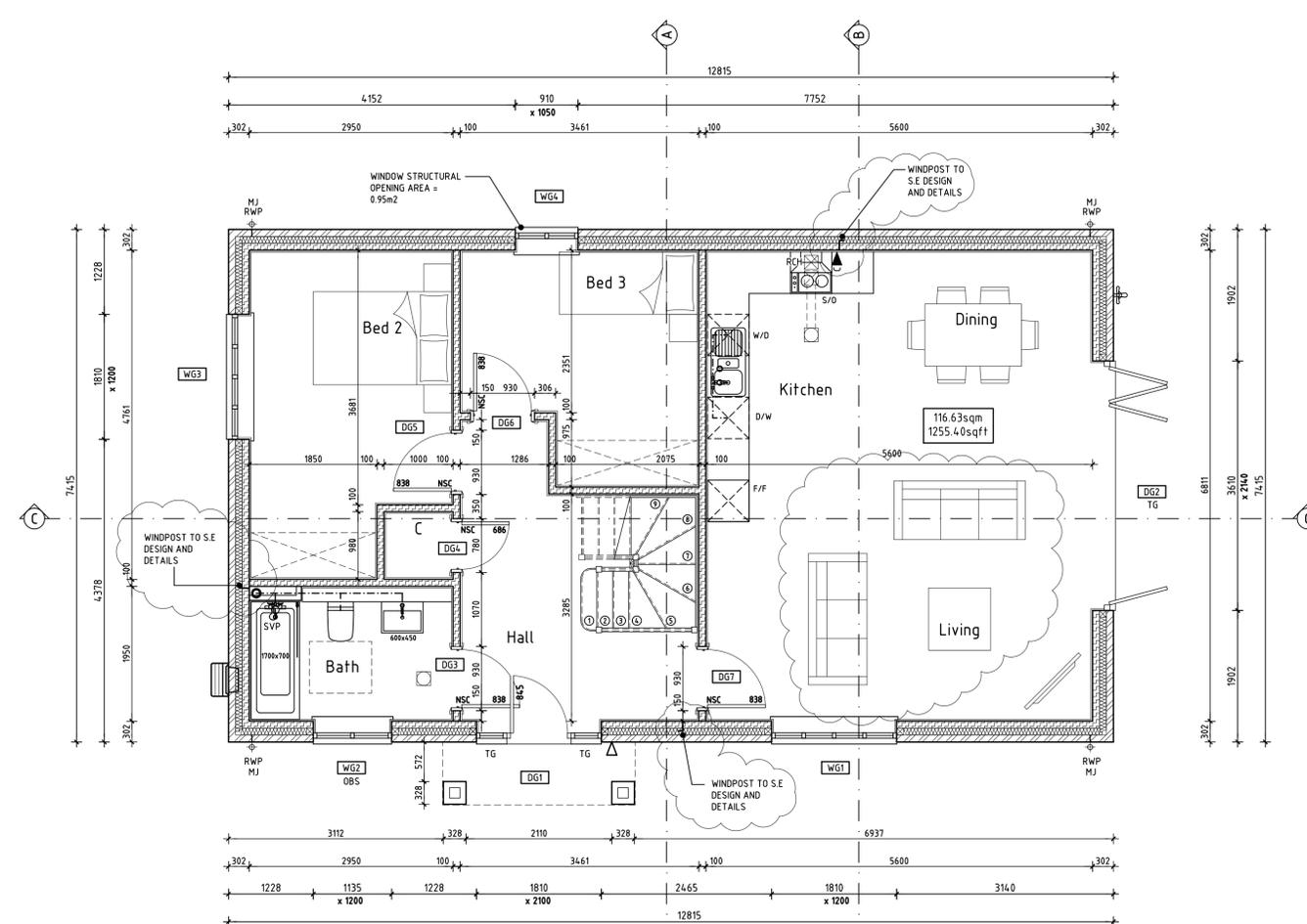
**Taylor Cox Associates**  
ARCHITECTURE & DESIGN

Churston House,  
Portsmouth Road,  
Esher,  
KT10 9AD  
Tel: 01372 466443  
Email: info@tcalimited.com  
www.tcalimited.com

PRIOR TO THE COMMENCEMENT OF ANY WORKS, THE BUILDER IS TO CHECK AND/OR DETERMINE ALL CONSTRUCTION DETAILS, INCLUDING CHECKING EXISTING SITE LEVELS AND DIMENSIONS. THE DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER PROJECT DRAWINGS, CONSTRUCTION NOTES AND/OR PROJECT SPECIFICATION. ALL DISCREPANCIES SHOULD BE REPORTED IMMEDIATELY.



FIRST FLOOR LAYOUT  
PLOT 1



GROUND FLOOR LAYOUT  
PLOT 1

**SUPERSTRUCTURE LEGEND**  
 ◊ RWP Rainwater downpipe  
 ○ SVP Soil Vent Pipe

**WALL LEGEND**  
 All blockwork strengths are to be confirmed by the S.E before any work commences on site. Refer to specification/S.E information for detailed technical requirements of wall types  
 [Hatched] 102.5 mm External facing brickwork  
 [Dotted] 7.3N Ext/Int blockwork.  
 [Dashed] 70mm metal stud  
 [Cross-hatched] 70mm metal stud insulated with 25mm Isover APR 1200 as indicated on plans

**SUPERSTRUCTURE NOTES**  
 1. Refer to specialist design for detailed kitchen layout.  
 2. All construction detailing to comply with 'Robust Details' guidance document where applicable.  
 3. All in accordance with S.E design and details. Refer to separate drawings for further information.  
 4. All construction detailing to be in accordance with the relevant accredited details listed in the construction specification.

**FIRE DOOR LEGEND**  
 S.C Door with self closing mechanism  
 N.S.C Door with no self closing mechanism

**SUPERSTRUCTURE ABBREVIATIONS**  
 OBS Obscure glass  
 TG Toughened glass  
 EE Emergency Egress Window  
 MJ Movement Joint  
 PS Padstones - refer to S.E design and details.

**VENTILATION LEGEND - SYSTEM 3**  
 Ventilation to be provided to comply with BR part F System 3 - continuously running independent extract fans. All in accordance with specialists design and details.  
 [Square] Ceiling mounted extract grille (refer to specialist design and details)  
 [Dashed] Notional ductwork route (to specialist design)

A 13.08.21 LIVING ROOM FURNITURE POSITIONS REVISED MN  
 FOLLOWING CREATION OF SERVICES LAYOUT.  
 WINDPOSTS ADDED TO SUIT CURRENT S.E DESIGN

Rev	Date	Details	Drawn

Client:

Site Address:  
 BRIGHTON ROAD,  
 TADWORTH

Description:  
 PLOT 1  
 GROUND AND FIRST FLOOR LAYOUTS

Status: TENDER

Date: JULY 21 Scale: 1:50@A1

Drawn: MN Checked: TCA

Job Number: 1296 Drawing Number: T/002 Revision: A

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Agenda Item 6

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# Agenda Item 7

Planning Committee  
24th November 2021

Agenda Item: 7 a and 7b  
20/02233/F & 21/01217/LBC

 <p><b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate</p>	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	24 NOVEMBER 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Matthew Lambert
	<b>TELEPHONE:</b>	01737 276659
	<b>EMAIL:</b>	matthew.lambert@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	7 a and 7b	<b>WARD:</b> <i>Reigate</i>

<b>APPLICATION NUMBER:</b>	a) 20/02233/F b) 20/01217/LBC	<b>VALID:</b>	12/11/2020
<b>APPLICANT:</b>	Star Pubs & Bars Mr David Wilbor	<b>AGENT:</b>	Design Id Ltd
<b>LOCATION:</b>	<b>THE BULLS HEAD PUBLIC HOUSE, 55 HIGH STREET, REIGATE, RH2 9AE</b>		
<b>DESCRIPTION:</b>	<b>External works to rear garden including the erection of an open sided timber pergola with felt roof finish, formation of a new decked area, installation of timber posts supporting stretched canvas sail to new decked area, construction of new external bottle bar timber shed.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

**This application is referred to Committee by Councillors Blacker and Whinney**

## SUMMARY

This is a full planning application and listed building consent for external works to the rear garden of the Bulls Head Public House. Consent is sought for three separate retrospectively built structures, all of which are presently in use; primarily a timber pergola, which lies directly to the rear of the public house building. This building would have a felt-roof finish, following the submission of amended plans. It measures 32.55m<sup>2</sup> internally. Its height is 2.8m, when measured from ground level, which is approximately 0.4m below the level of the adjacent eastern boundary fence.

To the south of this, planning permission is also sought for a freestanding shed, with corrugated metal roof. It has an entrance door to the north elevation, facing the public house building, and an openable serving hatch, which faces the residential dwellings to the western boundary. It is presently in use as an outdoor bar and service area.

# Agenda Item 7

Planning Committee  
24th November 2021

Agenda Item: 7 a and 7b  
20/02233/F & 21/01217/LBC

To the far south of the plot, a new decked area, measuring 51.5m<sup>2</sup> has been erected. Timber posts, elevated to a maximum height of 3.45m have been positioned and support a stretched canvas sail, allowing for the decked area to be covered. Following the submission of amended plans, new sections of fencing to the eastern, western and southern elevations would be fitted, covering the full height of the decked area. The decked area occupies a former area of beer garden, which has previously doubled as an informal area of parking for the landowner.

The public house on the site comprises a Grade II Listed Building and is situated within the Reigate Town Centre Conservation Area. The surrounding area is characterised by a mix of uses, but predominantly retail units. There are residential uses above the public house and to the rear of the site within Pilgrim Mews. 57B High Street lies directly to the rear of the plot. The land levels fall away to the rear. No significant trees would be impacted as a result of the proposal.

The principle issues in this case are the impact of the design of the proposal on the wider character of the Conservation Area and the Grade II Listed building, and the impact of the proposal on residential amenity. Various amendments, including increases to the height of the rear fencing to the raised deck area, and amendments to the roof of the pergola to reduce noise impacts, have been secured during the course of the application such that the proposal is now considered to strike an acceptable balance between the support of the wider use of the public house and garden, whilst preserving the amenity of neighbouring dwellings.

## **RECOMMENDATION**

Planning permission is **GRANTED** subject to conditions.

# Agenda Item 7

Planning Committee  
24th November 2021

Agenda Item: 7 a and 7b  
20/02233/F & 21/01217/LBC

## Consultations:

Conservation Officer: The proposals are acceptable from a listed building and Conservation Area point of view.

Archaeological Officer: No significant ground disturbance – no comments.

Society to protect Ancient Buildings – Provided no comments in respect of the proposal

Historic England – Do not consider that the consultation in respect of the application is necessary

Environmental Protection: Recommends a separate consultation to the planning noise consultants given the commercial nature of the site

Planning Noise/Environmental Health Consultants: Raise no objections to the scheme subject to conditions.

Ancient Monument Society – Provided no comments in respect of the proposal

The Georgian Group - Provided no comments in respect of the proposal

The Victorian Society - Provided no comments in respect of the proposal

Council For British Archaeology - Provided no comments in respect of the proposal

The Twentieth Century Society - Provided no comments in respect of the proposal

Reigate Business Guild - Provided no comments in respect of the proposal

Council For British Archaeology - Provided no comments in respect of the proposal

## Representations:

Letters were sent to neighbouring properties on 19 November 2020, and subsequently on 05 August 2021 and 14 October 2021.

A site notice in respect of the application was displayed on 30 November 2020

A press notice was advertised on 25 November 2020

Eleven letters of objection were received across the two applications. The following issues were raised –

### Issue

Intensification of use

### Response

See paragraph 6.9

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Light spill	See paragraph 6.11
Noise disturbance	See paragraph 6.10
Antisocial behaviour	See paragraph 6.12
Potential for amplified music	See paragraph 6.10
Potential for events	See paragraph 6.10
Pollution	See paragraph 6.10 & 6.11
Ecological Impacts	See paragraph 6.13
Crime Fears	See paragraph 6.12
Out of character with surrounding area	See paragraph 6.4 & 6.5
Overbearing relationship	See paragraph 6.15
Overshadowing	See paragraph 6.15
Overdevelopment	See paragraph 6.4 & 6.9
Overlooking and loss of privacy	See paragraph 6.14
Flooding	See paragraph 6.13
Impact upon parking/turning	See paragraph 6.16

## **1.0 Site and Character Appraisal**

- 1.1 The application site comprises a grade II listed public house, The Bulls Head, which is situated within the Reigate Town Centre Conservation Area. The Bulls Head is situated on the High Street and includes a beer garden to the rear. To the rear of the site, at basement level there is an enclosed yard area adjacent to the kitchen of the public house.
- 1.2 The surrounding area is characterised by a mix of uses, but predominantly shopping units. There are residential uses above the public house and to the rear of the site within Pilgrim Mews. The land levels fall away to the rear of the site.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: None, as the applicant did not approach the Local Planning Authority for pre-application advice prior to the submission of the application.
- 2.2 Improvements secured during the course of the application: Amended plans showing full-height fencing to the rear raised decking area on three elevations, alongside changes to the pergola roof from polycarbonate to felt.
- 2.3 Further improvements to be secured through conditions or legal agreement: Standard condition relating to approved plans, conditions relating to control of hours of use, provision and maintenance of suitable rear boundary fencing,

condition restricting live or amplified music in external areas, and events or functions in external areas

### 3.0 Relevant Planning and Enforcement History

3.1	90/01480/ADV	Display of Illuminated Signs	Refused	
3.2	12/00956/LBC	Retrospective application for installation of kitchen extract ducting at basement level of a public house	Approved Conditions	with
3.3	12/00971/F	Removal of the current two-storey rear extension and modern conservatory to the rear of the property and replace it with a more functional two-story extension	Refused	
3.4	12/00173/UA3	Enforcement enquiry regarding the installation of a flue	Case closed	
3.5	12/01857/F	Retrospective application for the installation of new kitchen extract at basement level to rear of the premises	Refused	
3.6	18/01899/F	Replacement of existing pergola to the immediate rear paved patio area with new to be constructed from 150mm x 150mm posts set into the ground with suitable cross members to form a decorative and more secure pergola. Part covered using acrylic corrugated sheets (to approximately 50% of the area). Existing festoon lights to be relocated to new pergola. Construction of new barbeque area to replace existing brick-built unit in approximately the same location as now. To provide a timber shed from which to serve bottled beers, consisting of 2400mm x 1800mm simple timber clad shed with galvanised tin roof and lockable service hatches front and side. Shed to be on a suspended timber floor and to be insulated. Construction of timber decking over	Application Returned	

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existing paved area to the bottom (south) of the garden and to extend across the hard-standing car park to the RHS (east) of the paved area. Decking to have a step up and balustrade/handrails to the exposed (west) edge and the front (north) edge. Installation of a canvas 'sail' shade over the decking supported from 150mm x 150mm posts and stretched on stretcher-bolts between the posts. All timber is to be softwood suitably treated with preservative, from sustainable and certifiable sources. The new sail will be a neutral colour canvas sail. The new barbeque will be formed using gabions

3.7	19/00273/UA3	Rear garden decked and covered without planning permission	DC Application received
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## 4.0 Proposal and Design Approach

4.1 This is a full planning application and listed building consent for the retrospective erection of a pergola, raised decking area and timber shed used as a bottle bar to the rear garden of the Bulls Head Public House.

4.2 Consent is sought for three separate retrospectively built structures, all of which are presently in use; primarily a timber pergola, which lies directly to the rear of the public house building. This building would have a felt-roof finish, following the submission of amended plans. It measures 32.55m<sup>2</sup> internally. Its height is 2.8m, when measured from ground level, which is approximately 0.4m below the level of the adjacent eastern boundary fence. To the south of this, planning permission is also sought for a freestanding shed, with corrugated metal roof. It has an entrance door to the north elevation, facing the public house building, and an openable serving hatch, which faces the residential dwellings to the western boundary. It is presently in use as an outdoor bar and service area. To the far south of the plot, a new decked area, measuring 51.5m<sup>2</sup> has been erected. Timber posts, elevated to a maximum height of 3.45m have been positioned and support a stretched canvas sail, allowing for the decked area to be covered.

4.3 A design and access statement has been submitted as part of the proposal. It should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to

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appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:

Assessment;  
Involvement;  
Evaluation; and  
Design.

4.4 Evidence of the applicant's design approach is set out below:

Assessment	<p>The proposed new timber structures, decked area and external bottle-bar timber shed will not be attached to any part of the Listed Building. Moreover, they will be hardly visible from the High Street as they will be located within the enclosed rear Beer Garden.</p> <p>This proposal does not appear to undermine any local planning policies.</p> <p>The proposed alterations are not detrimental to the current street scape and therefore we do feel that the proposal is in keeping with its surroundings and does not undermine any of the adjoining properties.</p>
Involvement	<p>Community views were not sought by the applicants. Pre-application advice was sought and provided by the council prior to the submission of the application.</p>
Evaluation	<p>No other development options were considered by the applicants.</p>
Design	<p>The applicant's grounds for the proposal was to improve the outlook of the beer garden, thus helping to entice new customers to the site and to make the business more viable.</p>

## 5.0 Policy Context

### 5.1 Designation

Urban Area

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS4 (Valued Townscapes and the Historic Environment)  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),

### 5.3 Reigate and Banstead Development Management Plan 2019

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DES1: Design of New development  
NHE9: Heritage assets

## 5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance

Surrey Design

Local Distinctiveness Design Guide,  
Reigate & Banstead Borough  
Council Local Character &  
Distinctiveness Design Guide  
Supplementary Planning Document  
Householder Extensions and  
Alterations SPG (2004)

Other

Human Rights Act 1998  
Community Infrastructure Levy  
Regulations 2010

## 6.0 **Assessment**

6.1 The main issues to consider are:

- Design appraisal / Impact upon the Listed Building and Conservation Area
- Neighbour amenity
- Community Infrastructure Levy

### Design appraisal / Impact upon the Listed Building and Conservation Area

6.2 The proposal seeks to carry out works within the curtilage of a Listed Building. The National Planning Policy Framework requires local planning authorities to have regard to any impact that a proposal may have on the character and setting of an identified heritage asset. As such the particular significance of the heritage assets in question needs to be assessed. Great weight should be afforded to the asset's conservation and the more important the heritage asset the greater that weight should be. Any harm to, or loss of, the significance of these heritage assets would require clear justification to be made. Harm to Grade II listed buildings should be exceptional. Within the local policy framework, Policy CS4 of the Core Strategy requires development to be designed sensitively to respect, conserve, and enhance the historic environment, including heritage and their settings. Policy NHE9 of the Council's Development Management Plan states that development will be required to protect, preserve, and wherever possible enhance, the Borough's designated and non-designated heritage assets and historic environment

including special features, area character or settings of statutory and locally listed buildings.

- 6.3 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high-quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.4 The proposal in terms of design, materials and dimensions is considered acceptable with regards to impact upon the setting of the Grade II Listed Building and Wider Reigate Town Centre Conservation Area. The overall visibility of the additions from the streetscene is low. To the west, they would be screened by the existing boundary fencing. Additional fencing would also be in place to screen the additions from vantage points to the east.
- 6.5 Given the site designations, The Council's Conservation Officer was consulted in respect of the proposals. It is his view that the proposals are acceptable from a listed building and Conservation Area point of view. Taking this into consideration, no objection is raised on design grounds.

### Neighbour amenity

- 6.6 Significant concern from neighbours have been raised during the consultation period citing harm as listed under the "Representations" section of this report. The beer garden at its closest proximity is located 4.7m from the boundary of the nearest property on Pilgrim Mews', no.6, and 14.7m from the rear windows of this dwelling. No's 7-9 Pilgrim Mews benefit from a slightly greater separation distance, the maximum being 11.4m and 19.1m respectively; in the case of no.9. To the rear, "Whitewood", no.57B High Street lies 1.2m from the rear of the site boundary.
- 6.7 Within the objections submitted, it is generally accepted that the public house, including the beer garden has functioned as such prior to the development of these neighbouring residential dwellings, and there is subsequently some support for the continued operation and vitality of the premises. The key concern lies with the purported intensification of the usage of the rear of the site, following the implementation of the raised decking and its enclosure by way of the fencing and canvas sail roof alongside the other additions. This section will address the key light and noise impacts. The remaining concerns in turn will also be commented upon.
- 6.8 The Department's Environmental Health Consultants were notified in respect of the proposal several times during the course of the application; most

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recently following the latest set of amended plans, which propose additional close boarded fencing to three elevations of the raised deck area, and replacement of the polycarbonate roof to the existing pergola with felt.

- 6.9 It was noted by the consultants that licensing restrictions already in place within the garden would have to inform any consideration of the application, with a key question being whether the proposal would materially intensify the use in a way which cannot be controlled by virtue of the licensing restrictions. Following consultation with The Council's Licensing department, it is noted that the present license ref. 18/00233/LAPREM was issued in March 2018. This license covers the entire premises and sets out operational hours in detail, therefore controlling the location within which all three elements under consideration are situated. As such, in order to limit conflict with separate legislation, and given that the licensed area would not change, it is not considered reasonable nor necessary to recommend planning conditions in respect of hours of use.
- 6.10 Concern has been raised with regard to the noise generated by the proposals. At present, the sections of close-boarded fencing to the south, east and west sides of the rear raised deck area are 1.8m above ground level. This leaves a gap of approximately 0.6m between the canvas sail and the top of the fencing. The Environmental Health Consultants were not satisfied initially with this arrangement and recommended that the fence be raised to 2.4m, in order that the 0.6m gap is filled. Amended plans to this effect were requested and received during the course of the application. Following this, the consultants have recommended conditions to secure details of the fencing, and its continued retention and maintenance. Further conditions were also recommended to prohibit live or amplified music, and/or live events in the rear garden. Following these conditions and taking the licensing restrictions as covered above into account, it is considered that a balance is struck between the usage of the rear garden, and sufficient controls to ensure that the proposals would not have an adverse impact upon the amenities of nearby residential dwellings.
- 6.11 Concern has also been raised with regard to the light spill generated by the proposal, particularly the canvas roof of the rear decking addition. The lux, and also the height of the lighting was cited within neighbouring objections. The Council's Environmental Health Consultants have noted with regard to this matter that the proposed lighting did not appear to be worrisome, subject to a condition to secure details of the lighting, specifically that it would comply with ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20, specifically E2 standards (suburban/village centre). It is considered that this condition would ensure that the light within the site is appropriately managed in a way that is suitable for the town centre location and considering the neighbouring residential dwellings.
- 6.12 Crime fears and antisocial behaviour have also been noted during the consultation period. It would appear that these issues have arisen as a result of the wider use of the site itself, and whilst developing the rear of the site, the

proposals in and of themselves have not directly resulted in an increase in antisocial behaviour. Issues of anti-social behaviour and criminal activity would be matters for the police and would not be material planning considerations.

- 6.13 Ecological issues and flooding have also been cited as causes for concern by residents. The site does not lie within a flood zone, nor an area of historic surface water flooding. Given the reasonably lightweight nature of the structures, the drainage is unlikely to have been subject to any significant impact. It is therefore considered that the material change in this regard is low. The site lies within the urban area, is reasonably small and is not host to any significant vulnerable open land or trees. The presence of protected species and habitats have not been identified within the site.
- 6.14 Concerns have also been raised with regard to overbearing, overshadowing and overlooking/privacy issues. It is noted that the raised decking area is situated in close proximity to no.57B. The raised fence of 2.4m in height would be sufficient to ensure that overlooking and privacy issues do not arise. The fencing on the eastern elevation would also prohibit any new views toward the neighbours on Pilgrim Mews to the east.
- 6.15 In terms of overbearing and overshadowing, it is considered that the proposals would be sufficiently spaced from the neighbours at Pilgrim Mews so as not to result in any harm. There would be separation of 1.2m from the rear of the raised decking and no.57B. This amenity relationship therefore comprises the main issue. At present, a wall measuring approximately 1.8m demarcates the boundary line between the application site and this neighbour. This wall lies around 1m from the front window of no.57B, which comprises the primary light source for a bedroom. With the addition of the fencing, this would increase to 2.4m. Given that the highest part of the wall lies above the highest part of the bedroom windows, the increase of 0.6m would not be so significant so as to harmfully reduce the present levels of light accessible to this window. Overall therefore the proposals are considered acceptable with regards the impact upon neighbour amenity.

### Highway Matters

- 6.16 The County Highway Authority were consulted in respect of the application, and note owing to the nature of the proposal, no particular view is given. The parking/turning matters in this case are therefore principally an amenity issue. It is understood that the area hosting the raised decking was formerly used as a parking area by the applicant. The property to the rear no.57B uses the area adjacent as parking. Objection has been raised by neighbours on the basis that the decking has reduced the turning circle for neighbours, therefore resulting in safety concerns, contrary to policy TAP1 of The Council's DMP. It has also been noted that the applicant is continuing to use neighbouring land not within their ownership for parking. It is noted that the proposals have been erected on land belonging to the applicant, and whilst the site may informally

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have been used by other landowners for turning, this is not a formal requirement, and would not form basis for refusal of the scheme. It is equally noted that the applicant should seek the permission of other landowners prior to using land which does not lie within the application site for parking or access.

## Community Infrastructure Levy (CIL)

- 6.17 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would not be CIL liable.

## **CONDITIONS - Application A: 20/02233/F**

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version Date</b>
Location Plan	11.81.PT 305	16/10/2020
Block Plan	11.81.PT 306	16/10/2020
Existing Plans	11.81.PT 302	16/10/2020
Existing Site Layout	11.81.PT 301	16/10/2020
Proposed Plans	11.081.PT.G.304	12/10/2021
Proposed Site Layout	11.081.PT.G.303	12/10/2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The raised decking hereby permitted shall not be used unless and until boundary fencing has been installed to provide screening in accordance with the height lines on the approved plans. Details of the full specification of the fencing shall be submitted to and approved in writing by the Local Planning Authority prior to first use. The approved fencing shall thereafter be permanently retained and maintained and there shall be no variation to its height without the prior approval in writing of the Local Planning Authority

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and to safeguard the amenity of neighbouring properties with regard to policy DES1 of the Reigate & Banstead Development Management Plan (September) 2019

3. There shall be no live or amplified music in external areas of the site

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Reason: To safeguard the amenity of neighbouring properties with regard to policy DES1 of the Reigate & Banstead Development Management Plan (September) 2019

4. There shall be no organised events or functions in external areas of the site

Reason: To safeguard the amenity of neighbouring properties with regard to policy DES1 of the Reigate & Banstead Development Management Plan (September) 2019

5. Within three months of the date of this permission the lighting installer shall provide written confirmation that the scheme shall comply with the LTL lighting design hereby approved and the recommendations for environmental zone E2 (suburban/village centre) in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20. Also within this period the applicant shall appoint a suitably qualified member of the institute of lighting professionals (ILP) to validate that the lighting scheme as installed conforms to the recommendations for environmental zone E2 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:20 and their report shall be submitted to and approved by the local planning authority within three further months.

Reason: To ensure that both the amenities of neighbouring properties are maintained with regard to policy DES1 of the Reigate & Banstead Development Management Plan (September) 2019.

## CONDITIONS - Application B: 20/01217/LBC

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version Date</b>
Location Plan	11.81.PT 305	16/10/2020
Block Plan	11.81.PT 306	16/10/2020
Existing Plans	11.81.PT 302	16/10/2020
Existing Site Layout	11.81.PT 301	16/10/2020
Proposed Plans	11.081.PT.G.304	12/10/2021
Proposed Site Layout	11.081.PT.G.303	12/10/2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

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## **INFORMATIVES - Application A: 20/02233/F**

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.org.uk](http://www.firesprinklers.org.uk).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. Should any development be proposed on or near the boundary then a party wall notice may be required under the Party Wall Act etc 1996.
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

## **REASON FOR PERMISSION – 7a and 7b**

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS10, CS11, DES1, NHE9 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

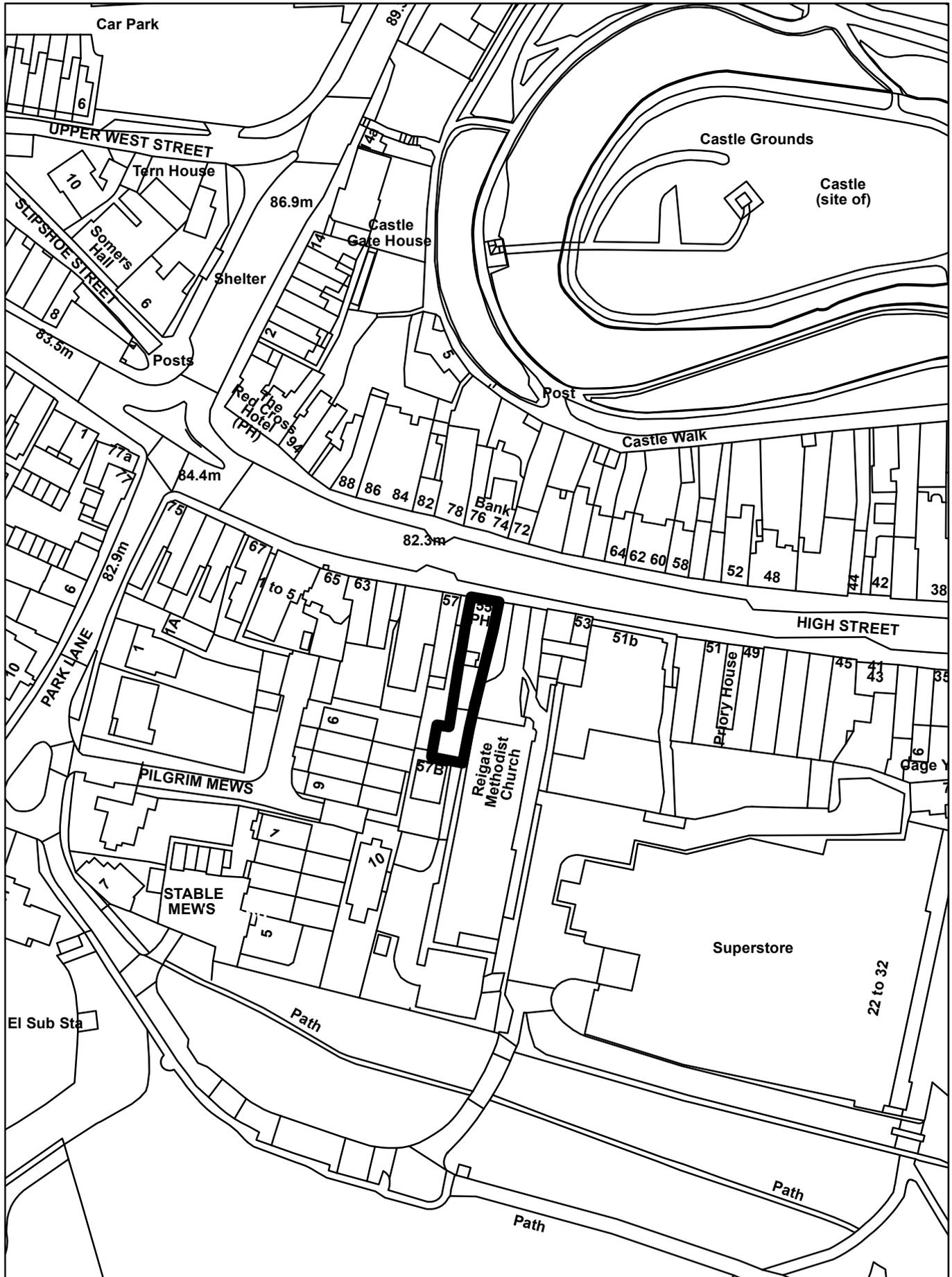
## **Proactive and Positive Statement – 7a**

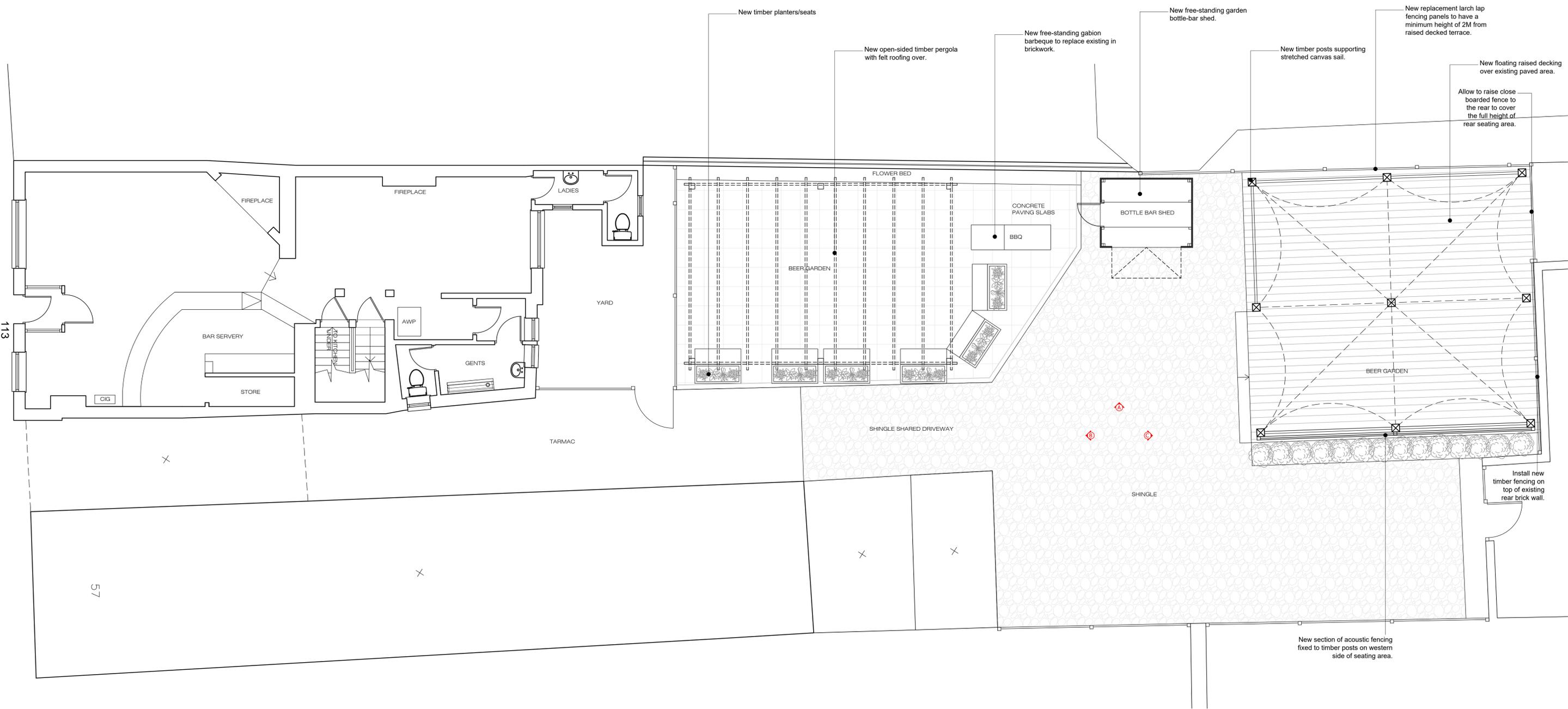
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

## **Proactive and Positive Statement –7b**

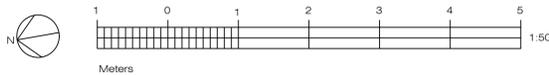
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant listed building consent in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

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20/02233/F & 20/01217/LBC - The Bulls Head Public  
House, 55 High Street, Reigate



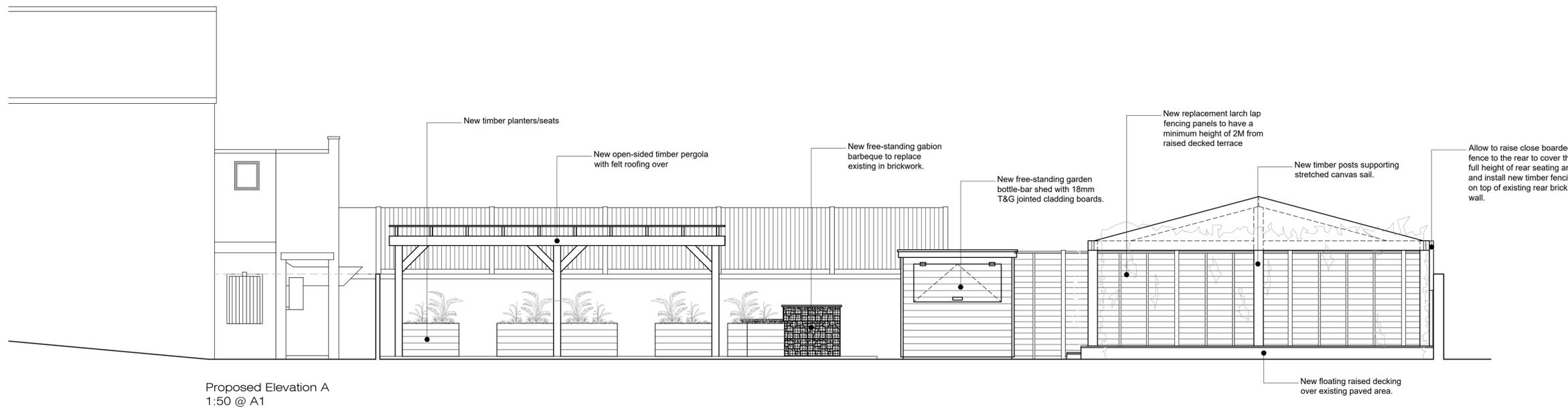


Proposed Plan  
1:50 @ A1

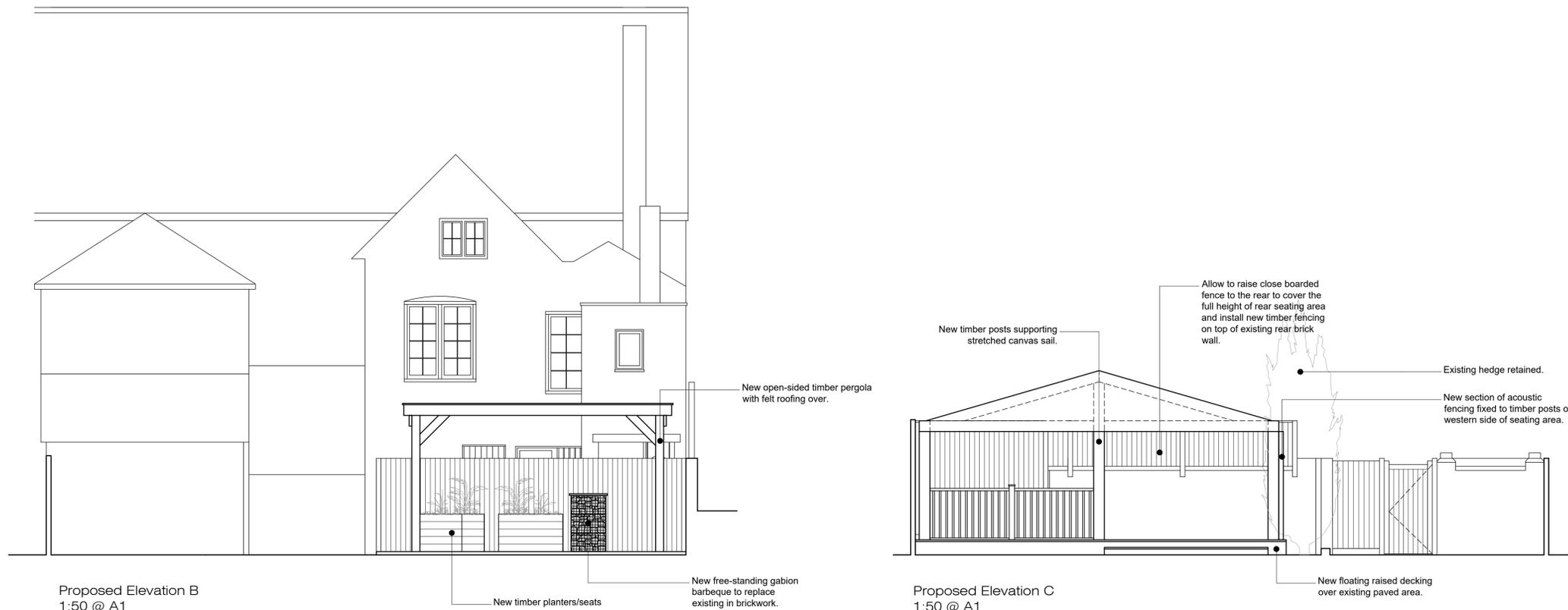


REVISIONS			
Rev.	Date	Details	Name
A	2021-05-04	Height of proposed replacement fence increased to 2M.	MT
B	2021-05-25	Notes revised.	MT
C	2021-07-13	Height of rear fence raise to cover the full height of the seating area to the rear. New fence on top of rear brick wall.	MT
D	2021-10-12	Roof finish of timber pergola changed to felt. Acoustic fencing added.	MT

	Client Star Pubs & Bars	Drawing Title Proposed Plan	Drawing No. 11.081.PT.G.303
	Project The Bull's Head 55 High Street Reigate RH2 9AE	Scale 1:50 @ A1	Status
	Drawn By	Date Sept 2020	<small>All dimensions to be checked on site. Any discrepancies between this drawing and other information is to be referred to the practice. This drawing is copyright.</small>

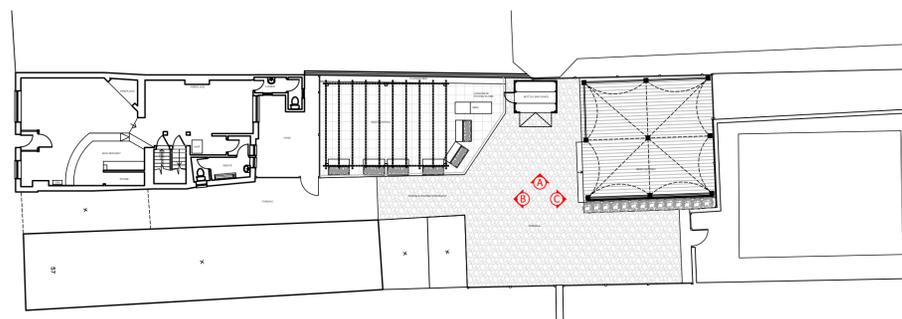


Proposed Elevation A  
1:50 @ A1

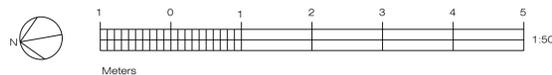


Proposed Elevation B  
1:50 @ A1

Proposed Elevation C  
1:50 @ A1



Key Plan  
1:200 @ A1



REVISIONS			
Rev.	Date	Details	Name
A	2021-05-04	Height of proposed replacement fence increased to 2M.	MT
B	2021-05-25	Height of proposed replacement fence increased to 2.3M.	MT
C	2021-07-13	Height of rear fence raise to cover the full height of the seating area to the rear. New fence on top of rear brick wall.	MT
D	2021-10-12	Roof finish of timber pergola changed to felt. Acoustic fencing added.	MT

	Client Star Pubs & Bars	Drawing Title Proposed Elevations	Drawing No. 11.081.PT.G.304
	Project The Bull's Head 55 High Street Reigate RH2 9AE	Scale 1:50 @ A1	Status
	Drawn By	Date Sept 2020	<small>All dimensions to be checked on site. Any discrepancies between this drawing and other information is to be referred to the practice. This drawing is copyright.</small>

# Agenda Item 8

Agenda Item: 8  
18/02159/F

Planning Committee  
24<sup>th</sup> November 2021

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	24 <sup>th</sup> November 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Matthew Sheahan
	<b>TELEPHONE:</b>	01737 276514
	<b>EMAIL:</b>	Matthew.sheahan@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	8	<b>WARD:</b> Horley West

<b>APPLICATION NUMBER:</b>	18/02159/F	<b>VALID:</b>	27/02/2019
<b>APPLICANT:</b>	Mr Darius Szymczyk	<b>AGENT:</b>	CP Planning Services Ltd
<b>LOCATION:</b>	<b>LAND PARCEL AT GOWER ROAD HORLEY</b>		
<b>DESCRIPTION:</b>	<b>Erection of 2 semi-detached houses. As amended on 27/02/2019 15/04/2019, 01/08/2019 and 13/07/2021</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the erection of a pair of semi-detached dwelling houses on land to the east of 1 Gower Road in Horley. The area is largely residential in character, comprising a mix of detached and semi-detached two storey dwellings, to the North and west, with a mix of flats and terraced housing to the east, with the majority of the land being within the ownership of Raven Housing Trust.

The land is currently an undeveloped parcel of open land but is not designated urban open land and therefore the principle of development is acceptable.

The proposed dwellings would be a matching pair, adopting a design approach that is consistent with that of existing dwellings on Gower Road and Drake Road beyond, adopting similar architectural features and a colour palette that would accord with the local vernacular.

The siting and separation distances in relation to neighbouring properties is considered to be sufficient to ensure that there would not be undue harm caused to the amenity of 1 Gower Road to the west and 1-3 Thornton Plan to the south.

The County Highway Authority (CHA) has been consulted on the proposal and are satisfied that the level of parking provision would be acceptable and that vehicles can safely enter and exit the dwellings without resulting in a highway safety issue, particularly in view of the adjacent traffic island on the corner of Gower Road and Court Lodge Road.

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## **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.

## Consultations:

Highway Authority: Following the submission of amended plans, the County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority has

Horley Town Council: No objection. The Town Council queries whether this plot of land has been designated as Urban Open Space under the Development Management Plan.

SES Water: No objection received

## Representations:

Letters were sent to neighbouring properties on 10<sup>th</sup> January 2019. Additionally a site notice was posted on 11<sup>th</sup> January 2019. A total of 4 responses were received. Following the receipt of amended plans further consultation took place on 5<sup>th</sup> March 2019. A further 3 responses were received. Following the submission of further amendments re-consultation took place on 18<sup>th</sup> April 2019 and a further 2 responses were received. The consultation responses raised the following issues:

<b>Issue</b>	<b>Response</b>
Overdevelopment	See paragraphs 6.3-6.4
Alternative location/ proposal preferred	See paragraphs 6.3-6.4
Harm to Conservation Area	See paragraphs 6.10-6.14
Overlooking and loss of privacy	See paragraphs 6.8-6.9
Overbearing	See paragraphs 6.8-6.9
Loss of private view	See paragraphs 6.8-6.9
Increased noise and disturbance	See paragraph 6.15
Inconvenience during construction	See paragraph 6.15
Drainage/ sewerage capacity	See paragraphs 6.10-6.14
Health/ crime fears	See paragraphs 6.10-6.14
Harm to countryside/ greenbelt	See paragraphs 6.10-6.14
Harm to wildlife habitat	See paragraphs 6.10-6.14
Inadequate parking	See paragraphs 6.17-6.19
Increased traffic/ congestion	See paragraphs 6.17-6.19
Hazard to highway safety	See paragraphs 6.17-6.19

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## **1.0 Site and Character Appraisal**

- 1.1 The site comprises a parcel of land to the east of number 1 Gower road in Horley, on the corner of Gower Road and Court Lodge. The area is largely residential in character, comprising a mix of detached and semi-detached two storey dwellings, to the North and west, with a mix of flats and terraced housing to the east, with the majority of the land being within the ownership of Raven Housing Trust.
- 1.2 The development of Gower Road took place during the 1990s, with dwellings exhibiting a pleasant yet plain design, with properties generally displaying pitched roofs, with a mix of red or yellow facing brickwork, small amounts of render to the ground floor porches, with minimal architectural detailing. Flatted development to the south and east of the site is older and more dated in architectural style.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: The applicant did approach the Council for pre-application advice prior to the submission of the current planning application.
- 2.2 Improvements secured during the course of the application: Amended plans have been received following concerns raised regarding the land ownership of existing parking spaces on the site. Further amendments were submitted to address concerns raised by the Highway Authority in regard to parking provision and access to and from these spaces on to the adjacent highway. Improvements to materials were also sought and have been submitted.
- 2.3 Further improvements could be secured: Improvements to the scheme could be secured by way of suitably worded conditions.

## **3.0 Relevant Planning and Enforcement History**

There is no planning or enforcement history relating to the proposed application site.

## **4.0 Proposal and Design Approach**

- 4.0 This is a full application for the erection of a pair of semi-detached dwellings on an area of open land to the east of 1 Gower Road. It is proposed that they would be a matching pair, adopting a design approach that is consistent with that of existing dwellings on Gower Road. The dwellings would be approximately 8m in height, with an eaves height of 5.4m. Both properties would be 8.3m in depth from front to rear. Materials would be consistent with that of neighbouring properties by incorporating dark coloured facing brickwork to the ground and first floors, with a light coloured render to the

porch. The front elevations would be consistent with the front building line established along the south side of Gower Road.

- 4.1 Each dwelling provides 4 bedrooms, with the fourth bedroom occupying the roof space. They would have a footprint of approximately 62 sq. m, providing approximately 131sq. m of residential floor space over three floors. Each would be provided with a rear garden, separated by a mix of boundary fencing and planting. These gardens would have a maximum depth of 7.8m.
- 4.2 Each dwelling would be provided with one car parking space. New dropped kerbs would be created allowing access on to areas of hardstanding in front of both dwellings, which would be softened by strips of landscaping.
- 4.3 Further details of the development are as follows:

Site area	0.04 hectares
Existing use	Open Land
Proposed use	C3 Dwelling House
Proposed Site Density	50 d.p.h (dwellings per hectare)
Existing parking spaces	0
Proposed parking spaces	2
Parking standard	2 spaces per 4 bedroom dwellinghouse

## 5.0 Policy Context

### 5.1 Designation

Urban area

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS4 (Valued Townscapes and Historic Environment)  
CS10 (Sustainable Development)  
CS11 (Sustainable Construction),

### 5.3 Reigate & Banstead Borough Development Management Plan 2019

Design	DES1 DES2 DES4 DES5 DES6 DES8
Movement	TAP1 Annex 4 Parking Standards
Climate change resilience and flooding	CCF1
Natural and Historic Environment	NHE3

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## 5.4 Other Material Considerations

National Planning Policy Framework  
National Planning Practice Guidance  
Supplementary Planning Guidance

Surrey Design  
Local Distinctiveness Design Guide  
A Parking Strategy for Surrey  
Parking Standards for Development

Other

Human Rights Act 1998  
Community Infrastructure Levy  
Regulations 2010

## 6.0 **Assessment**

6.1 The application seeks permission for the erection of two semi-detached dwellings. The land is not designated as urban open space and therefore the principle of development is accepted and Policy OSR1 is not relevant.

6.2 The main issues to consider are:

- Design and Impact on the character of the area;
- Amenity of future occupiers;
- Neighbour amenity;
- Trees and Landscaping;
- Highway Impact, Access and parking
- Sustainability, Infrastructure and Climate Change
- Affordable Housing;
- CIL

### Design and Character

6.3 The site is open to grass and does not have any significant positive impact upon the area's character. The design of the two dwellings has been informed by the character of the surrounding area. Properties along the southern side of Gower Road are relatively subdued in terms of their appearance, utilising simple brickwork with modest detailing in the form of brick banding, soldier bricks above the window heads, with a number displaying small, pitched roofs above first floor windows. This design has been echoed in this proposal. Whilst the ridge of the proposed dwellings would exceed the height of its' neighbouring property to the west, 1 Gower Road is untypically small and therefore something of an exception. The proposed dwellings would not exceed the building heights set by properties beyond. The fenestration would match those of other dwellings in the road.

- 6.4 There would be a gap of 2.6m between 1 Gower Road and the nearest proposed dwelling, which is considered to be acceptable. This would exceed what is typical of spacing in Gower Road, meaning that no terracing effect would occur within the road. There would be sufficient distance to the rear boundaries for properties which are of a fairly modest depth, ensuring that the development would not appear cramped. On this basis it is considered that the two dwellings would be acceptable with regard to design and impact on the character of the road and surrounding area and would accord with Policy DES1 of the Development Management Plan 2019 (DMP) in this regard.

### Amenity for future occupants

- 6.5 It is a fundamental objective of planning policy and stated within the National Planning Policy Framework 2021 that we provide high quality housing that is well designed and built to a high standard. The advice is amplified further by policies DES2 and DES5 of the Development Management Plan, which requires developments to demonstrate that dwellings have been designed to ensure that a good standard of amenity for all existing and future occupants and meet the minimum relevant nationally described space standards and be arranged to ensure that habitable rooms are arranged to have an acceptable outlook and where possible receive direct sunlight. These standards require a two storey two bedroom property to a minimum of 70 sqm of residential floorspace. This would be more than accommodated by the proposed dwelling, which would be approx. 130sqm.
- 6.6 It is considered the floor layouts and level of living space provided would be acceptable. Each property would provide four bedrooms, including one within the roof space, with each habitable room being well served by windows providing light and outlook.
- 6.7 Externally each dwelling would have a rear garden, ranging between 75 and 90 sq. metres in area. Rear garden space varies within the locality, ranging from as small as 35 sq. m for properties along Drake Road to the west, with some properties to the north along Gower Road exceeding 100 sq. metres. The proposal would fit comfortably within this range and would be acceptable. It is considered that the proposal would be acceptable with regard to amenity for future occupants and would comply with Policy DES5 in this regard.

### Neighbour Amenity

- 6.8 Numbers 1-3 Thornton Place are located to the south of the application site, at angle to the application site, with the front elevations facing the proposed dwellings. A strip of hedging immediately in front of a public footpath separates the two sites. A gap of approximately 14m would separate the two facing elevations. Due to the angle at which 1-3 Thornton Place would face the proposed dwellings, the opportunity for direct overlooking from window to window would be difficult. The separation distance would be sufficient to ensure that the development would not be overbearing in nature. The visual presence of built form would be subdued by the presence of a mature hedge to the rear boundary.

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- 6.9 Turning to 1 Gower Road, the proposed development would extend approximately 2.4m beyond the neighbours' rear elevation, with a separation distance of approximately of approximately 4.2m between flank elevations. Whilst the presence of built form would result in a change in the relationship with the neighbouring property, the dwellings would not significantly interfere with levels of light allowed to enter rear facing windows serving habitable rooms. There are no side facing windows on either the neighbouring property or the proposed, therefore there would not be the opportunity for direct overlooking. The separation distance would be sufficient to avoid any overbearing impact occurring. It is considered that the proposed development would comply with Policies DES1 of the DMP2019 with regard to impact on neighbour amenity.
- 6.10 Within objections, concern has been raised that the development would result in the loss of a private view. Whilst the immediate outlook of a neighbouring property can be taken in to account, there is not a right to a private view that can be considered during the assessment of a planning application. It is not considered that in this case there would be any resultant harm to the immediate outlook of any neighbouring properties.
- 6.11 Further concern is raised on the grounds that the development would result in harm to the Green Belt and a Conservation Area. An assessment of the area has concluded that the site does not fall within either of these areas and has therefore not been considered.
- 6.12 No protected species have been identified as being present on site. There is separate legislation in place that would come into force in the event of such species being identified.
- 6.13 Issues regarding impact on the existing drainage and sewerage capacity would be considered at the building regulations stage.
- 6.14 Issues concern crime fears would be a matter for the police, whilst health fears would not be a matter for the planning authority for consideration. No specific existing or potential issues have been identified in this respect. Similarly, whilst health fears have been cited by interested parties, no specific issues have been identified in this respect. The effect of the development on crime and health is considered to be no different to any other small residential scheme.
- 6.15 On the issue of increased noise and disturbance and inconvenience during construction, this is a temporary inconvenience and a necessary consequence of development that does not constitute a reason to refuse planning permission.

## Trees and Landscaping

- 6.16 At present the site is a piece of open land which is grassed over bordered by mature hedging to the rear and partially to the east side boundary. There are

no trees within the site. It is not proposed to remove any existing vegetation from the site. The submitted site plan indicates the intention to include small areas of landscaping to the front of each of the plots. In the event of planning permission being granted, and to ensure that the proposed landscaping is acceptable, a condition would be attached requiring the submission of a landscaping scheme to the LPA for approval prior to the commencement of development. Further conditions requiring the submission of both a tree protection plan and boundary treatment details would also be required. Subject to the above outlined conditions the proposed scheme is deemed to be acceptable in regard to tree impact and landscaping.

### Highway Impacts, Access and Parking

- 6.17 The site is located within an area of medium accessibility as defined within the Council's Parking Standards. On this basis four parking spaces would be expected to be provided, two for each dwelling. This requirement would be met by providing two spaces to the front of the dwellings and two further spaces to the side, accessed via Thornton Place.
- 6.18 There is currently an area of hardstanding to the western side of the site which is within the ownership of 1 Gower Road. During the course of the application further information was requested by the CHA in order to clarify how each of the proposed dwellings would be accessed from the front. The plans as originally submitted did not demonstrate how both dwellings would be accessed. Concern was initially raised that access to the dwelling to the east of the site should not present a highway safety issue in respect of the proximity to a traffic island immediately in front of the site.
- 6.19 To address these issues amended plans were submitted that demonstrate two separate vehicle crossovers, one for each dwelling, would be provided, leading to an area of hardstanding for each dwelling. The dwelling to the east would be provided with a larger parking area so that it is possible that a vehicle can both enter and exit the site in a forward gear. The crossover would be sited a sufficient distance away from the traffic island that the CHA does not consider that there would be a resultant highway safety issue in this regard. The CHA has recommended conditions in the event of planning permission being granted in regard to access and parking layouts and the provision of electric charging points. On this basis the application is considered to be acceptable with regard to highway matters and would accord with Policy TAP1 of the DMP 2019 in this regard.

### Sustainability, Infrastructure and Climate Change

- 6.20 Policy CCF1 of the Council's Development Management Plan 2019 seeks to ensure that all new development contributes to reducing carbon emissions. New development will be encouraged to incorporate passive and active energy efficiency measures and climate change resilience measures and renewable energy technologies. In order that the proposed development contributes to achieving these aims, in the event that planning permission were to be granted, conditions requiring demonstration that it will meet the

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national water efficiency standard of 110litres/person/day and achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations would be attached. A further condition requiring the provision of broadband connection, in accordance with Policy INF3 of the DMP 2019, would also be attached to any grant of planning permission.

## CIL

- 6.21 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although, the exact amount would be determined and collected after the grant of planning permission.

## Affordable Housing

- 6.22 Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, the 2018 NPPF makes clear such contributions should not be sought from developments of 10 units or less.
- 6.23 In view of this the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

## **CONDITIONS**

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Proposed Plans	GOWER-RD-PROPLN	A	13.07.2021
Elevation Plan	GOWER-RD-PROELE	A	13.07.2021
Elevation Plan	GOWER-RD-PROELE	A	13.07.2021
Site Layout Plan	GOWER-RD-PROSITE	A	13.07.2021
Location Plan	GOWER-RD-BLK-LOC	A	13.07.2021
Proposed Plans	GOWER-RD-SIGH	A	13.07.2021
Site Layout Plan	GOWERRD-PROSITE-020419	B	13.07.2021

## Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall take place until samples of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Policy DES1 of the Reigate and Banstead Borough Development Management Plan 2019

4. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A, B and C of Part 1 of the Second Schedule of the 2015 Order shall be constructed.

Reason: To control any subsequent enlargements in the interests of the visual and residential amenities of the locality with regard to Policy DES1 of the Reigate and Banstead Borough Development Management Plan 2019.

5. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason: To ensure that the development does not affect the amenity of the neighbouring properties by overlooking and to protect the visual amenities of the area in accordance with Policy DES1 of the Reigate and Banstead Borough Development Management Plan 2019

6. No development shall commence on site until a scheme for the landscaping of the site including the retention of existing landscape features has been submitted to and approved in writing by the LPA. Landscaping schemes shall include details of hard and soft landscaping, including any tree removal/retention, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation and management programme.

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All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved or in accordance with a programme agreed in writing with the local planning authority.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

## Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies DES1 and NHE3 of the Reigate and Banstead Borough Development Management Plan 2019

7. No part of the development shall be first occupied unless and until the proposed vehicular access to Gower Road has been constructed in accordance with the approved plans and thereafter shall be kept permanently maintained.

Reason: In order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policy TAP1 of the Reigate and Banstead Development Management Plan 2019 and the objectives of the NPPF 2021.

8. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked Thereafter the parking areas shall be retained and maintained for its' designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users in order to meet the objectives of the NPPF 2021, and to satisfy policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

9. The development hereby approved shall not be occupied unless and until both dwellings are provided with a fast charge socket (current minimum requirement 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users in order to meet the objectives of the NPPF 2021, and to satisfy policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

10. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:

- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
- b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

11. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:

- A) A broadband connection accessed directly from the nearest exchange or cabinet
- b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#) .
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the

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exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.

4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate substantial sized trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Semi-Extra Heavy Standard size with initial planting heights of not less than 4m with girth measurements at 1m above ground level in excess of 14/16cm.
6. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned

wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

7. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
8. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice).
9. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.

## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against the NPPF 2019 and Development Management Plan policies DES1 DES2 DES4 DES5 TAP1 NHE3 CCF1 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the

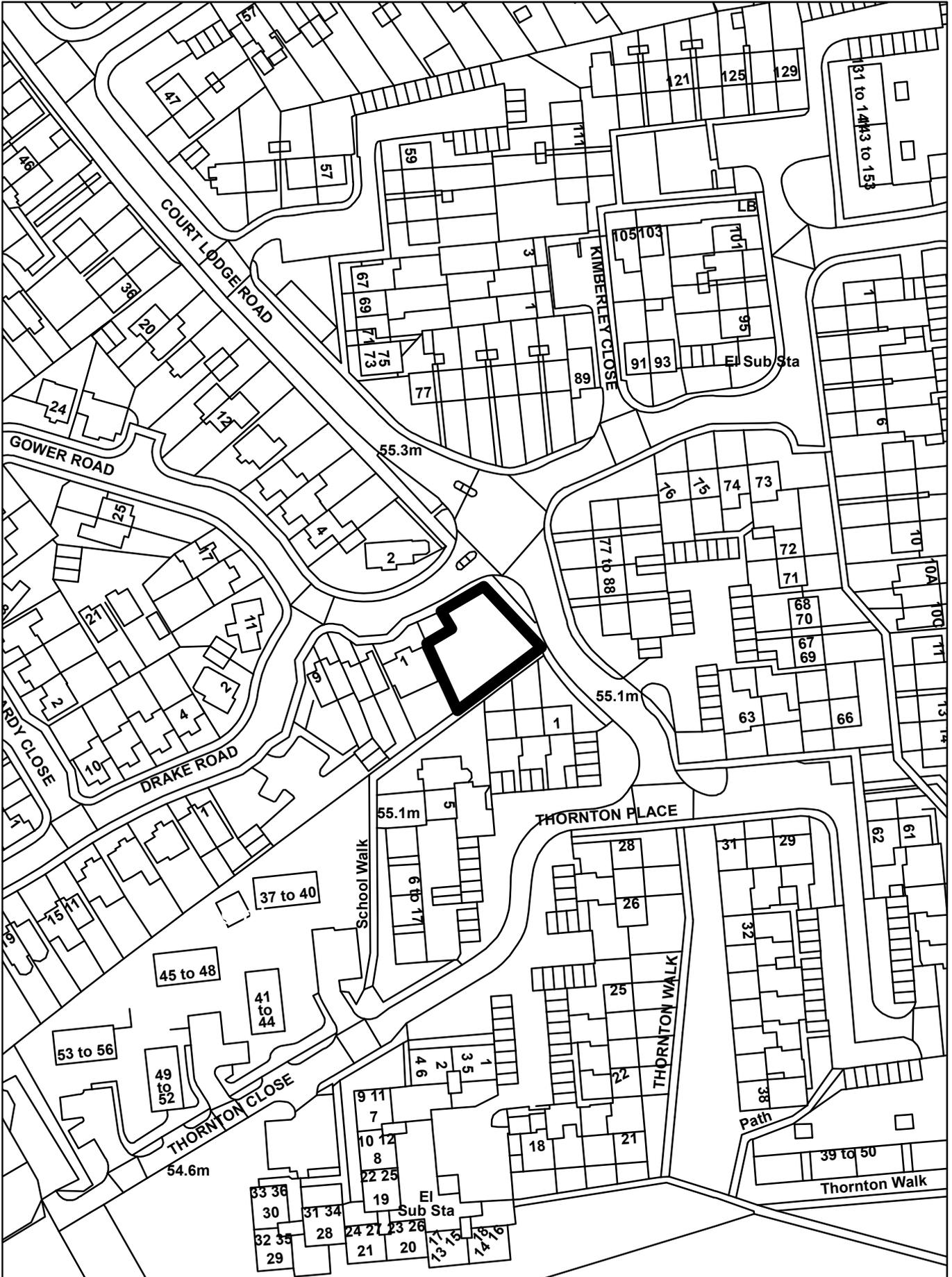
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presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

18/02159/F - Land Parcel At 527442 143303,  
Gower Road, Horley



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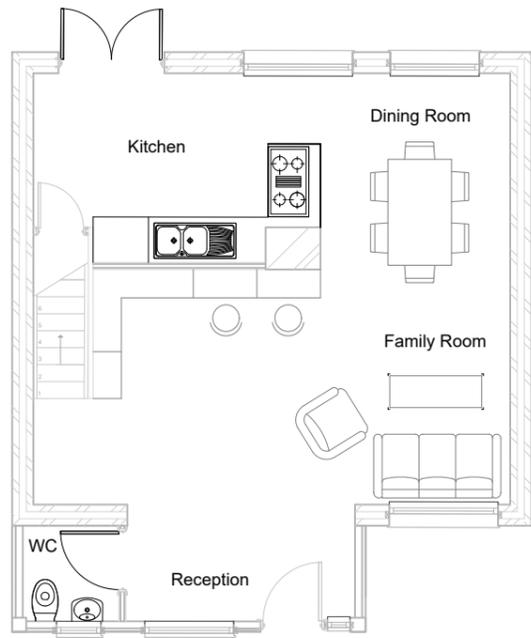
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CHRISTOPHER.PETERS@CPPLANNING.COM

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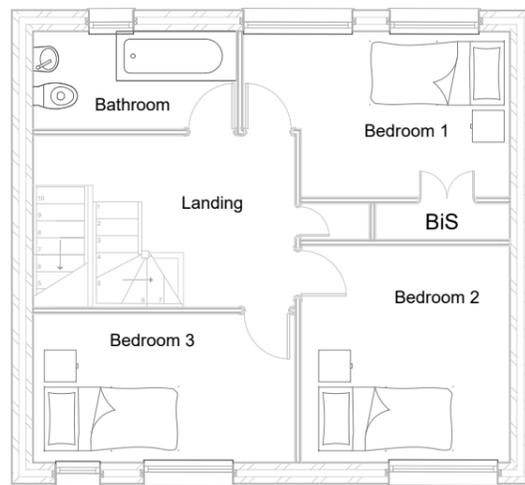
1. The works shown have been drawn for submission to the local planning authority and are not to be used for construction.
2. No works shall commence until planning approval has been given by the local authority.
3. No works shall commence without full plans building control approval or a building notice served with the local authority.
4. The dimensions indicated are approximate and subject to site checks by the contractor prior to commencement.
5. CP Planning services accept no responsibility for any unknown factors which either preclude the works from construction or add additional costs, ie. location of public sewers and underground services, abnormal foundation designs, due to trees, ground conditions or any underground structure or obstruction, condition of the existing building



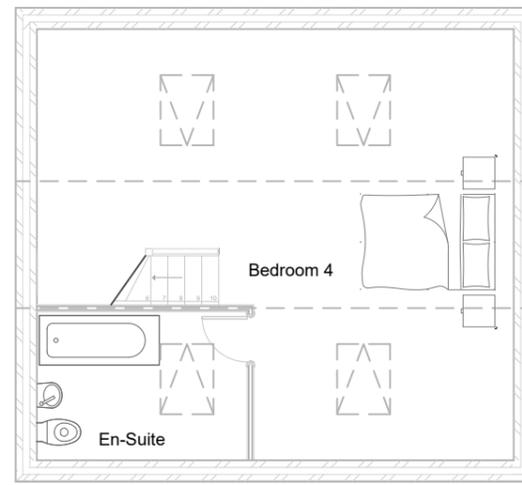
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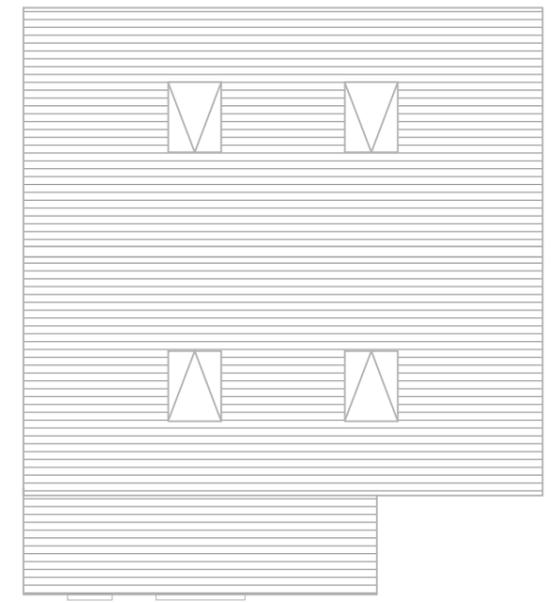
Proposed ground floor



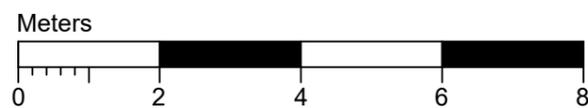
Proposed first floor



Proposed second floor

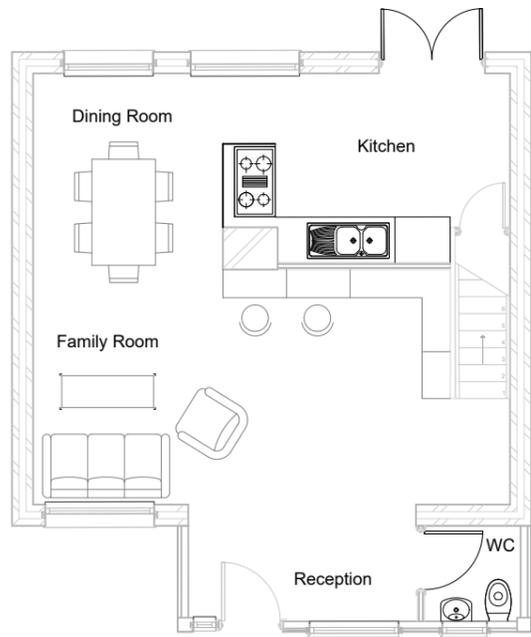


Proposed roof plan

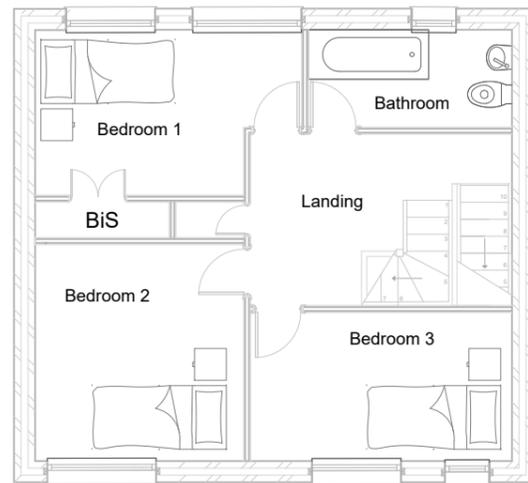


Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
PROJECT <b>GOWER-ROAD HORLEY RH6-8SH</b>			
DRAWING TITLE <b>PROPOSED-PLANS DWELLING-I</b>			SPEC SPEC AREA AREA
SCALE 1:100	DATE 27/06/2021	DRAWN AT	
DWG NO. <b>GOWER-RD-PROPLN</b>	REV. <b>A</b>		

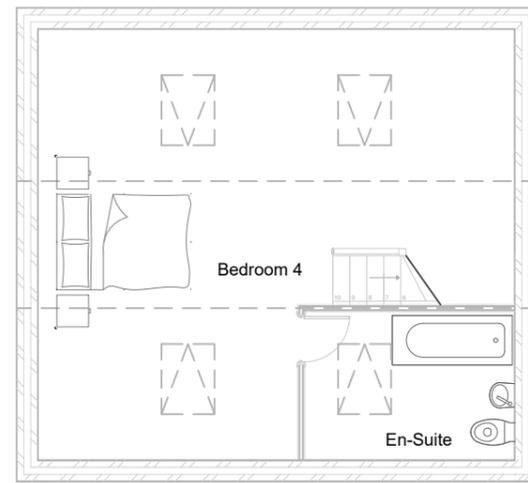
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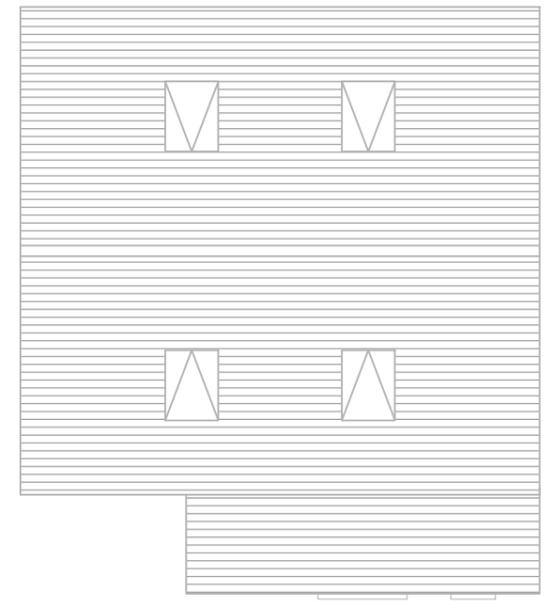
Proposed ground floor



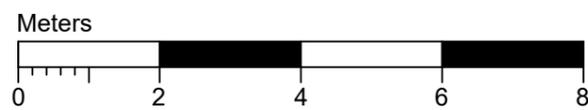
Proposed first floor



Proposed second floor



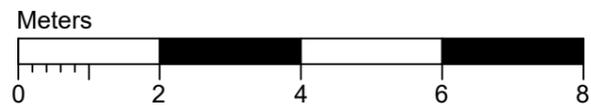
Proposed roof plan



Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
PROJECT <b>GOWER-ROAD HORLEY RH6-8SH</b>			
DRAWING TITLE <b>PROPOSED-PLANS DWELLING-2</b>			SPEC SPEC
			AREA AREA
SCALE 1:100	DATE 27/06/2021	DRAWN AT	
DWG NO. <b>GOWER-RD-PROPLN</b>			REV. <b>A</b>

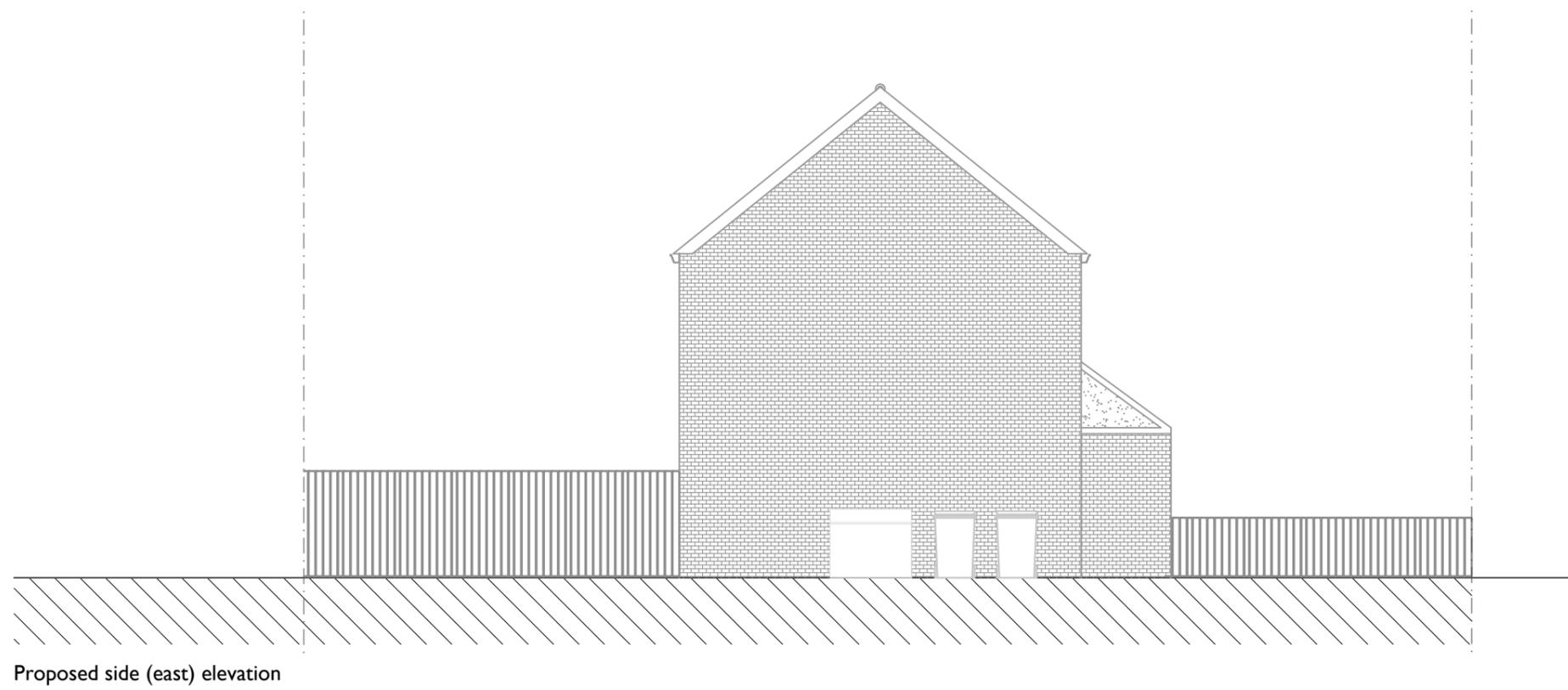


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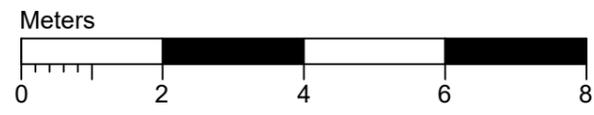


Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
PROJECT <b>GOWER-ROAD HORLEY RH6-8SH</b>			
DRAWING TITLE <b>PROPOSED ELEVATIONS</b>			SPEC SPB AREA AR
SCALE 1:100	DATE 27/06/2021	DRAWN AT	
DWG NO. <b>GOWER-RD-PROELE</b>	REV. <b>A</b>		

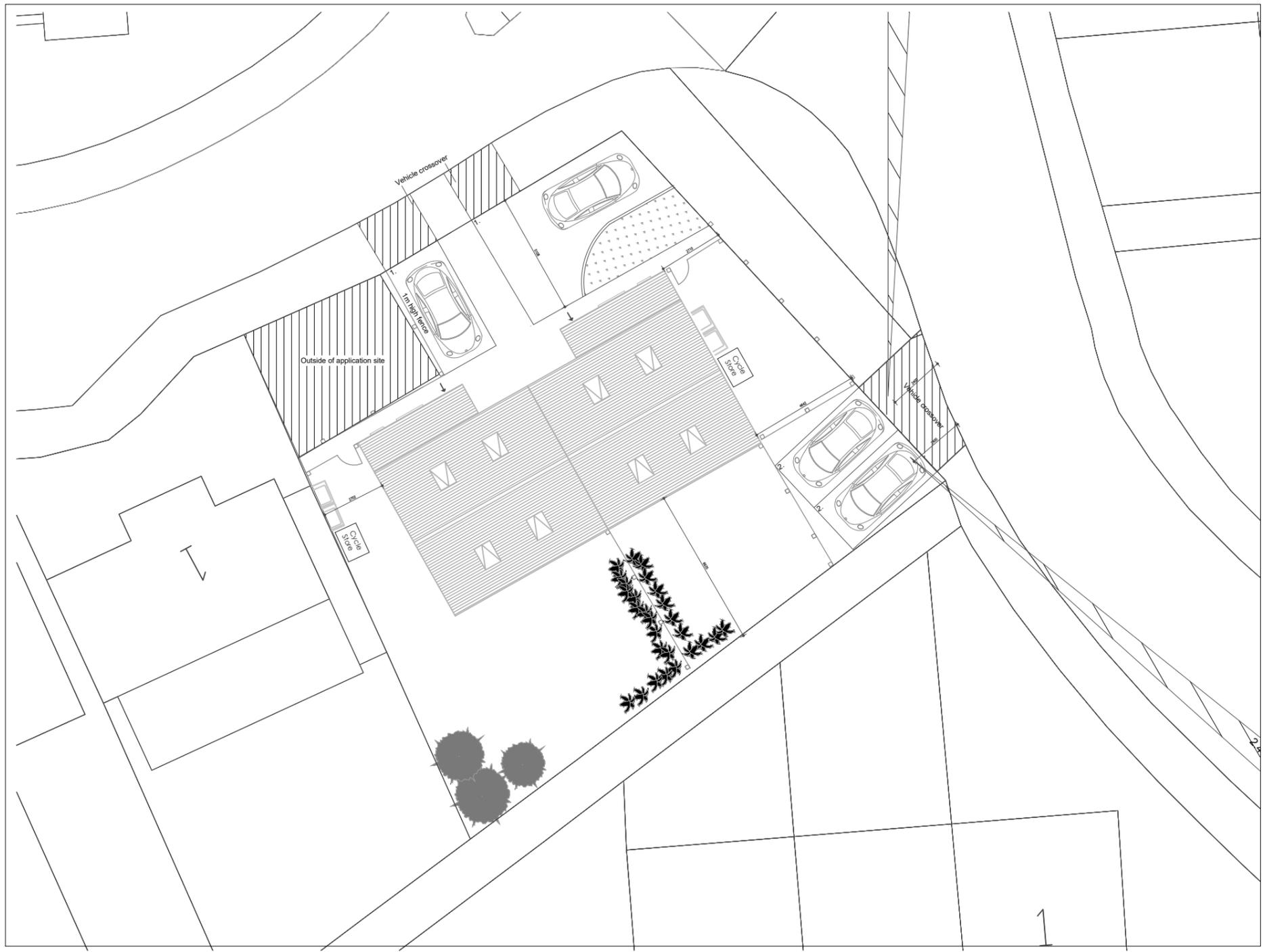
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Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
PROJECT <b>GOWER-ROAD HORLEY RH6-8SH</b>			
DRAWING TITLE <b>PROPOSED ELEVATIONS</b>			SPEC SPEC AREA AREA
SCALE 1:100		DATE 27/06/2021	DRAWN AT
DWG NO. <b>GOWER-RD-PROELE</b>			REV. <b>A</b>



Proposed site plan (1:200)



Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
PROJECT <b>GOWER-ROAD HORLEY RH6-8SH</b>			
DRAWING TITLE <b>PROPOSED SITE-PLAN</b>			SPEC SPB AREA ARB
SCALE 1:200	DATE 27/06/2021	DRAWN AT	
DWG NO. <b>GOWER-RD-PROSITE</b>	REV. <b>A</b>		

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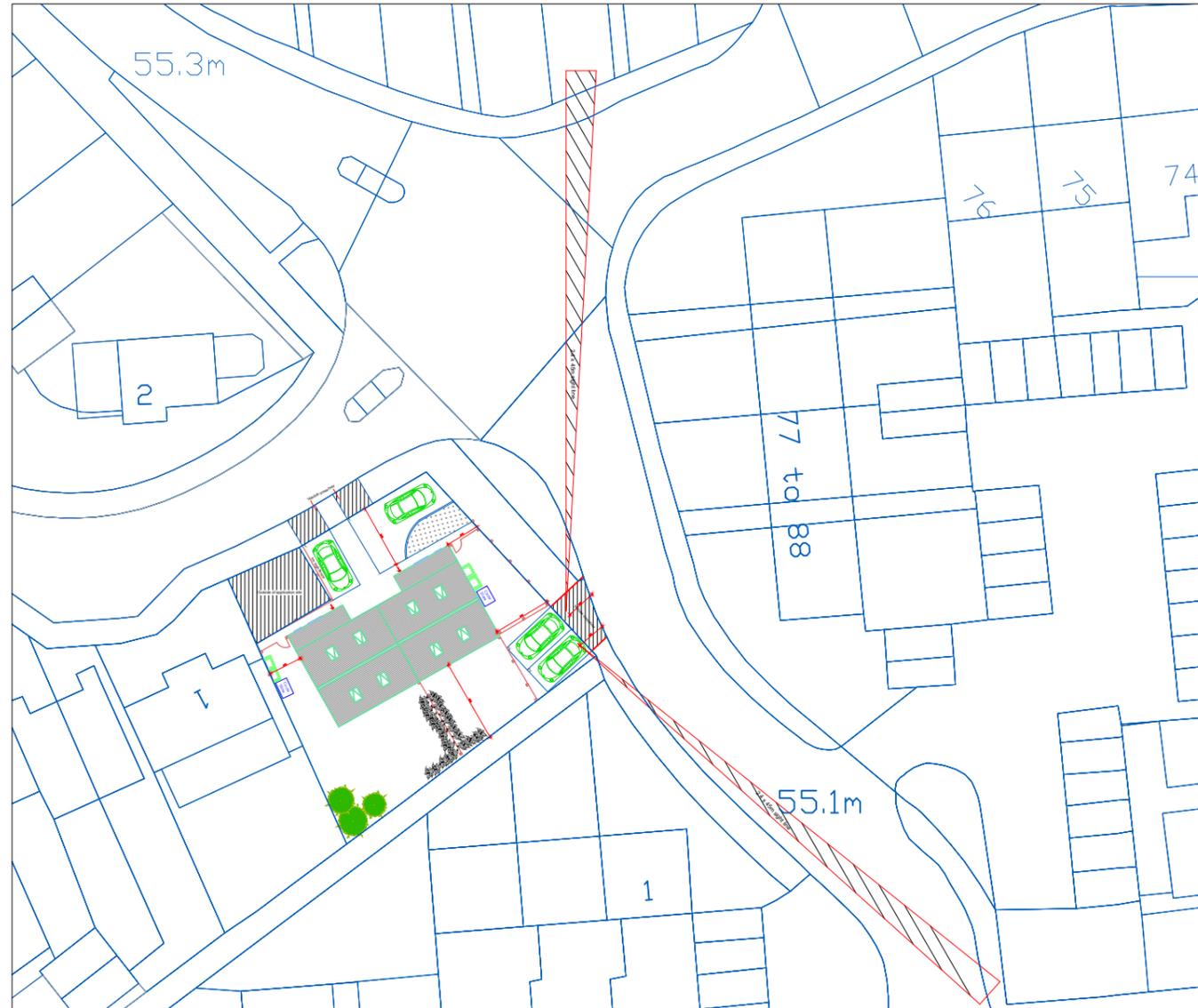
Block plan (1:500)



Location plan (1:1250)



Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
<b>PROJECT</b>			
GOWER-ROAD HORLEY RH6-8SH			
<b>DRAWING TITLE</b>			<b>SPEC</b>
BLOCK-PLAN LOCATION-PLAN			SPEC
			<b>AREA</b>
			AREA
<b>SCALE</b>		<b>DATE</b>	<b>DRAWN</b>
1:500:1250		27/06/2021	AT
<b>DWG NO.</b>			<b>REV.</b>
GOWER-RD-BLK-LOC			A



Sight lines plan (1:500)



Rev	Date	Notes	Initials
-	-	-	-
CP Planning Services		230 SMALLFIELD ROAD, HORLEY RH6 9LT	
<b>PROJECT</b>			
GOWER-ROAD HORLEY RH6-8SH			
<b>DRAWING TITLE</b>			<b>SPEC</b>
SIGHT-LINES			SPE AREA AR
<b>SCALE</b>		<b>DATE</b>	<b>DRAWN</b>
1:500		27/06/2021	AT
<b>DWG NO.</b>			<b>REV.</b>
GOWER-RD-SIGHT			A

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# Agenda Item 9

Planning Committee  
24 November 2021

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21/02537/ADV

 <b>Reigate &amp; Banstead</b> <b>BOROUGH COUNCIL</b> Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	24 November 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Clare Chappell
	<b>TELEPHONE:</b>	01737 276004
	<b>EMAIL:</b>	Clare.Chappell@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	9	<b>WARD:</b> Redhill East

<b>APPLICATION NUMBER:</b>	21/02537/ADV	<b>VALID:</b>	18/10/2021
<b>APPLICANT:</b>	Harlequin Theatre and Cinema	<b>AGENT:</b>	-
<b>LOCATION:</b>	<b>HARLEQUIN THEATRE 16 LONDON ROAD REDHILL SURREY RH1 1NN</b>		
<b>DESCRIPTION:</b>	<b>Vinyl wrapping of a staircase, glass and on windows at the atrium of the harlequin theatre</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

This application is referred to Committee because the applicant is Reigate & Banstead Borough Council.

## SUMMARY

This is an advertisement application made under the Advertisement Regulations for adverts on the glass balustrade of the staircase leading up to the Harlequin Theatre and library, and adverts on the glazed doors of the foyer.

The adverts would increase awareness of the Harlequin facilities and the public library, and serve as directional signs to improve accessibility from street level at the front and from the car park to the rear. Their colour, size, siting, and overall design are considered proportionate and appropriate for the advertising of public facilities by a Local Authority, and the proposal would not adversely affect visual amenity or public safety.

## RECOMMENDATION(S)

Planning permission is **GRANTED** subject to conditions.

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## Consultations:

Redhill Town Centre Partnership: No response.

Contaminated Land Officer: No response.

Theatres Trust: The signage will help further raise visibility and awareness of the theatre. It will not add to the building's footprint or require illumination. Therefore it would not cause harm to the amenity of surrounding properties and neither do we consider it would impact on highway safety. Therefore we would support the granting of advertisement consent.

## Representations:

Letters were sent to neighbouring properties on 19 October 2021. No representations had been received by the end of the 21-day consultation period.

### 1.0 Site and Character Appraisal

- 1.1 The application site comprises the Harlequin Theatre, the adjoining foyer area and external staircase on London Road. The staircase is large, with two flights of stairs from each direction of London Road. There is a central landing with a further flight of stairs leading to the foyer within a glazed atrium. The foyer serves the library and the Harlequin Theatre (including cinema and café) at the first-floor level of the building. The foyer can also be accessed from the car parks to the rear.
- 1.2 The site lies within the town centre of Redhill. The area is typified by shops and other town centres uses. There are some residential flats on the opposite side of the road. London Road, along with much of the town centre of Redhill is pedestrianised.

### 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: The opportunity did not arise because the applicant did not approach the Local Planning Authority before submitting the application.
- 2.2 Improvements secured during the course of the application: None.
- 2.3 Further improvements to be secured through conditions or legal agreement: Standard advert conditions.

### 3.0 Relevant Planning and Enforcement History

- 3.1 17/00460/F - Infilling of the ground floor colonnade along London Road and Station Road (511sqm) and the conversion and extension of the existing toilet to form a new retail unit (115sqm). Approved with conditions. (not implemented)

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- 3.2 16/02388/ADV - 2 no. new internally illuminated suspended theatre signs. Express consent.
- 3.3 13/00168/S73 - Construction of a Class A1 use retail food store of 15,093sqm gross external floorspace, a hotel, a gym, a multi storey car park of 927 spaces, general townscape improvement and associated works. Variation of condition 1 of permission ref no 11/00212/F seeking an amendment to reduce the size of the hotel to 70-bedrooms. Granted
- 3.4 12/01852/F - Construction of a Class A1 use retail food store of 14,666 sqm (GEA), gymnasium, multi-storey car-park, general townscape improvements and associated works. Granted
- 3.5 11/00212/F Construction of a Class A1 use retail food store of 15,093sqm gross external floorspace, a hotel of 98 rooms, a gym, a multi storey car park of 927 spaces, general townscape improvement and associated works. Granted.
- 3.6 97/12530/ADV - Display of 2 internally illuminated cinema advertising signs. Approved with conditions.
- 3.7 93/06420/ADV - The Harlequin Warwick Quadrant Redhill. 'Moving message' display units. Approved with conditions.
- 3.8 92/00930/ADV - The Harlequin N E Quadrant Redhill. Retention of various signs at the harlequin theatre. Approved.
- 3.9 90/00850/ADV - Harlequin Theatre Warwick Quadrant Redhill. Display of projecting signs for harlequin theatre.
- 3.10 88/16920/DMD - The Harlequin Theatre Civiv Hall Warwick Quadrant Redhill. Proposed dormer type window in managers office. Approved.
- 3.11 86/09610/ADV - Warwick Quadrant Redhill. Display of advertising boards relating to 'the harlequin' theatre. Granted.
- 3.12 85/06850/F - London/Station Road Warwick Quadrant Redhill. Pedestrian areas of the Warwick Quadrant forming part of the paved/pedestrian areas of the Redhill town centre.the proposals include paving bricks glazed canopies lights fixtures trees seats. Granted.
- 3.13 83P/0286 - London Road, Redhill. Staircase to serve the civic hall & library with glazed barrel vaulted roof over part of the proposed pedestrianised section of London Road. Approved with conditions.
- 3.14 80P/1347 - London Road, Station Road, and part of Ladbroke Road public car park (now Warwick Quadrant) landscaping and paving areas, unloading lay-by, clocktower and upper parts of north-east quadrant development not otherwise covered by proposals within re.78p/1349. Approved with conditions.

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- 3.15 78P/1349 - Mixed development comprising shops, offices (174,000 sq ft), car parking, civic hall and library, public lavatories and bus station on land bounded by London Road, Station Road and Ladbrooke Road, Redhill. Approved with conditions.

## 4.0 Proposal and Design Approach

- 4.1 This is an advertisement application made under the Advertisement Regulations for adverts on the glass balustrade of the staircase leading up to the Harlequin Theatre and library, and adverts on the glazed doors of the foyer.
- 4.2 The applicant has submitted proposed images and layouts in order to illustrate the proposed signage and it is clarified that a design and access statement is not required for an application for advertisement consent.

## 5.0 Policy Context

### 5.1 Designation

Urban Area

Primary Shopping Area

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)

CS4 (Valued Townscapes and Historic Environment)

CS7 (Town and Local Centres)

### 5.3 Reigate and Banstead Development Management Plan

DES1 (Design of new development)

DES10 (Advertisements and shop front design)

### 5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Town & Country Planning (Control of Advertisements) (England)

Regulations 2007

Supplementary Planning Guidance

Shop Front & Shop Sign  
Design Guide 2001

Other

Human Rights Act 1998

The Department for Communities  
and Local Government (2007)  
Outdoor advertisements and signs:  
a guide for advertisers

## 6.0 Assessment

- 6.1 The proposal seeks advertisement consent to install vinyl signs/adverts on the street-facing side of the staircase balustrade and on the glazed doors of the foyer.
- 6.2 The main issues to consider are:
- Visual amenity
  - Public safety

### Visual Amenity

- 6.3 The Control of Advertisement Regulations stipulates advertisements are considered in terms of how their appearance would impact the general characteristics of the locality. The NPPF states (under the "Achieving well-designed places" section, para 136) that the quality and character of places can suffer when advertisements are poorly sited and designed and that control over advertisements should be operated in a way which is simple, efficient and effective. Advertisements should be subject to control only in the interests of amenity and public safety, taking account of cumulative impacts.
- 6.4 The proposed adverts/signs would all be vinyl stickers (made from plastic with an adhesive for sticking to glass). All the vinyls proposed in this case would result in an opaque appearance. It is understood that the vinyl product has a lifespan and is removable using a heating process, but the vinyl product is intended for permanent adverts/signs, as opposed to something which can be periodically changed.
- 6.5 The adverts on the glass balustrade of the staircase would fit the individual glass panels leaving a clear border around each. This would enable multiple adverts and signs including 'theatre', 'cinema', 'library' 'Harlequin', 'café', 'room hire'. The background colour would be dark blue, and the text and illustrations would be in white and cream. Individually, each panel is not very large with maximum size of vinyl being 1.0m by 0.3m approx., and so the lettering would not be overly large, especially when compared to ordinary shopfront signage in the locality. On mass, the 25no. panels would create impact, because of the introduction of the dark blue next to the existing light grey and glass finishes on the staircase. Given the intended purpose of directing people towards and making the public aware of the theatre, library etc. which are not visible from the street level, the arrangement is considered to be proportionate and appropriate.
- 6.6 There are several sets of doors in the foyer area which would receive similar vinyl adverts; these would also have a dark blue background with white and cream text/illustrations. The London Road entrance to the foyer comprises two pairs of double doors and a pair of automatic doors. The automatic doors would remain clear of adverts. The two pairs of push/pull doors and the fixed glazed panels either side would receive adverts to their lower halves. The layout of the adverts would be symmetrical to match the symmetry of the glazing. The rear car park entrance to the foyer only contains one set of automatic doors and again, these would remain clear of adverts. The fixed glazed panels either side would receive the adverts to

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their lower halves. The adverts would only face the exterior of the building and the interior view would display the light grey coloured backing material of the vinyls.

- 6.7 The entrance to the Harlequin itself comprises a pair of assisted doors and a single manual door. Above the doors are glazed panels. The larger panel would have a sign which reads 'The Harlequin' with 'Theatre & Cinema Redhill' beneath and the smaller panel would read 'welcome'. On the interior side these panels would read 'thanks for visiting! See you again soon'.
- 6.8 By virtue of using vinyls on the glazed areas only, the structural framing of the atrium and doorways would not be disrupted, and so the adverts would not undermine the architectural proportions of the foyer area. The original sign for the harlequin entrance is a projecting one at high level; it is modest in size and appearance, and perhaps not so easy for new visitors to see. The proposed signage, particularly above the entrance doors to the Harlequin, would increase visibility and would also be modest in size compared to typical cinema/theatre signage.
- 6.9 In summary, the adverts and signage are considered to be appropriate in terms of their impact on the visual amenity of the locality and so the proposal complies with policies DES1 and DES10.

## Public Safety

- 6.10 Taking into account the siting, appearance and scale, the advertisements are not considered to result in harm to public safety or traffic safety. Concerns were raised by members about the glazing being obscured on the lower half of the door, and that this creates a risk of doors opening onto children. It is considered that this does not constitute a material public safety issue in planning terms.

## **CONDITIONS**

1. No advertisement is to be displayed without the permission of the owner of the site or any other person with an interest in the site entitled to grant permission.

Reason: To comply with Regulation 6(1) and Schedule 2 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

2. No advertisement shall be sited or displayed so as to: (a) endanger persons using any highway, railway, waterway, dock, harbour, or aerodrome (civil or military); (b) obscure, or hinder the ready interpretation of, any traffic sign, railway signal or aid to navigation by water or air; or (c) hinder the operation of any device used for the purpose of security or surveillance or for measuring the speed of any vehicle.

Reason: To comply with Regulation 6(1) and Schedule 2 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

3. Any advertisement displayed, and any site used for the display of advertisements, shall be maintained in a condition that does not impair the visual amenity of the site.

Reason: To comply with Regulation 6(1) and Schedule 2 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

4. Any structure or hoarding erected or used principally for the purpose of displaying advertisements shall be maintained in a condition that does not endanger the public.

Reason: To comply with Regulation 6(1) and Schedule 2 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

5. Where an advertisement is required under the Town and Country Planning (Control of Advertisements) (England) Regulations 2007 to be removed, the site shall be left in a condition that does not endanger the public or impair visual amenity.

Reason: To comply with Regulation 6(1) and Schedule 2 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

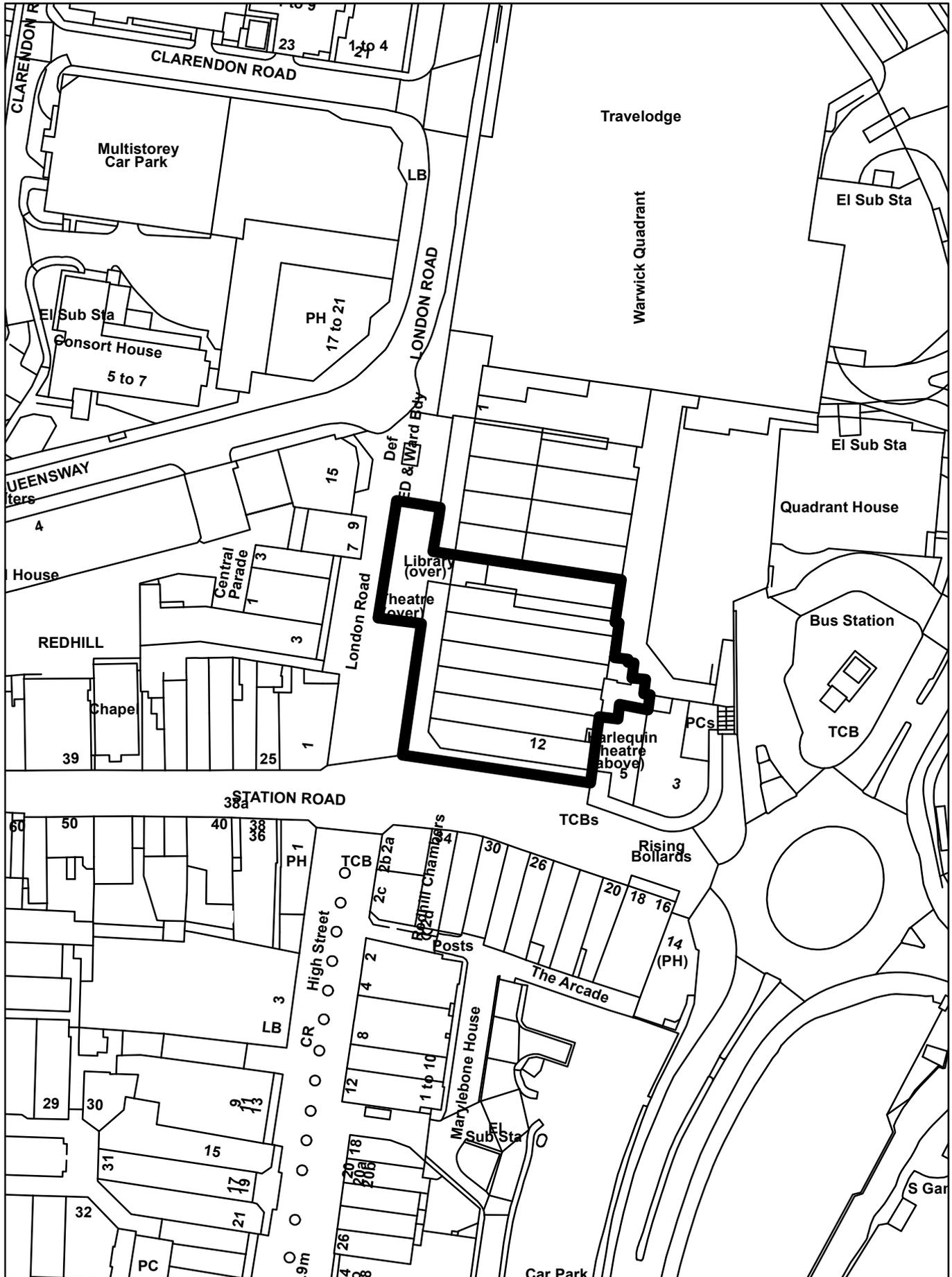
## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS7, DES1, DES10 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework

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 21/02537/ADV - Harlequin Theatre, 16 London Road,  
 Redhill



# A



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E



D



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**Happy to proceed?**  
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